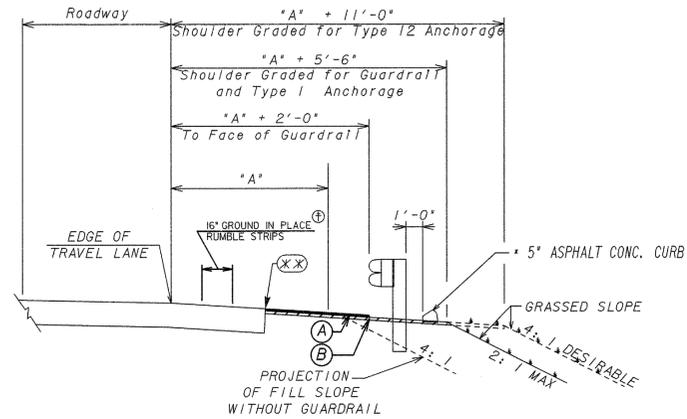


NO SCALE
 CLASS "B" CONCRETE BASE OR PAVEMENT WIDENING
 Item Code 500-9999 - Cu. Yds.

In excavated areas that are 5'-0" or less in width, Class "B" concrete may be placed in lieu of the base and paving specified by the typical section. Payment will be made under "Class B Concrete Base and Pavement Widening".

In excavated areas greater than 5'-0" in width, the Contractor shall place base and paving as specified on the typical section.

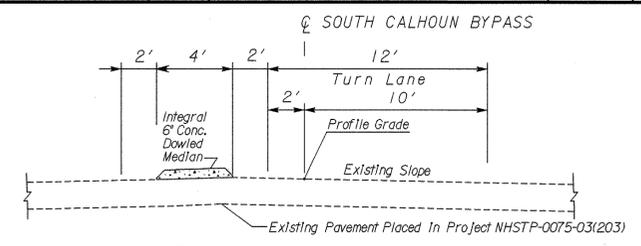


TYPICAL SHOULDER DETAIL FOR GUARDRAIL AND ASPHALT SHOULDER
 SEE PLAN FOR LOCATION

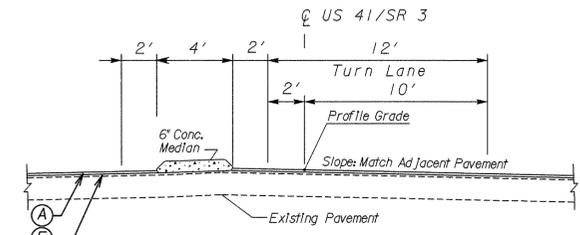
STA 270+37.50 AND STA 283+25.00 LT.
 NO ASPHALTIC CURB SHALL BE INSTALLED BEHIND GUARDRAIL FOR WATER QUALITY.

SEE GA DETAIL D-26 FOR DETAILS
 SEE GA STD 4387 FOR DETAILS
 SEE GA CONSTRUCTION DETAIL S-9 FOR DETAILS

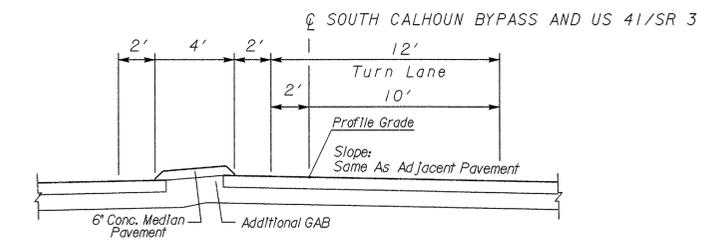
- REQUIRED PAVEMENT**
- (A) RECYCLED ASPH CONC 12.5 MM SUPERPAVE, GP 2 ONLY, INCL BITUM & H LIME (165 LBS/SY)
 - (B) RECYCLED ASPH CONC 19 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME (220 LBS/SY)
- (X) EXCLUDE SAFETY EDGE TREATMENT AT ASPHALT SHOULDER LOCATIONS



DETAIL FOR INTEGRAL MEDIAN TURN LANE
 STA 318+96.34 TO 320+65.00
 STA 352+80.00 TO 354+73.49



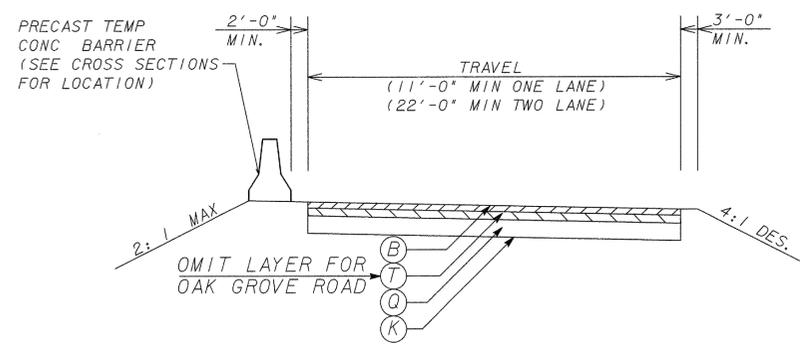
DETAIL FOR INTEGRAL MEDIAN TURN LANE
 STA 101+58.00 TO 105+56.00 US 41/SR 3



DETAIL FOR MEDIAN TURN LANE
 SEE PLAN FOR LOCATION
 CONCRETE PAVEMENT SHOWN, ASPHALT SIMILAR

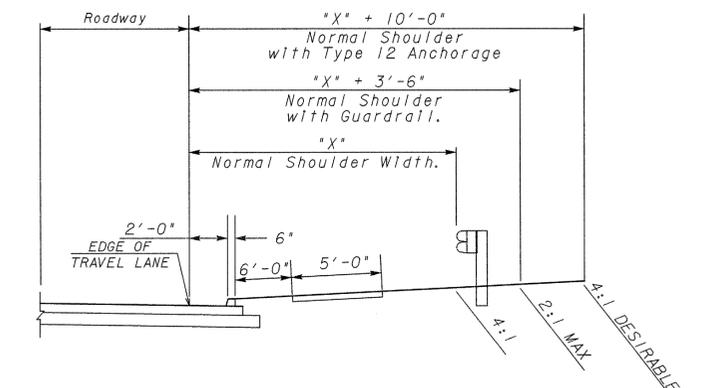
FOR THIS PROJECT, CROSS SLOPES THAT ARE ADJUSTED TO "BEST FIT" EXISTING PAVEMENT SLOPES ARE SUBJECT TO THE FOLLOWING LIMITS:

- A. NORMAL CROWN
- | SECTION WITH GRADES 0.5% OR GREATER | SECTION WITH GRADES LESS THAN 0.5% |
|-------------------------------------|------------------------------------|
| 1.5% - MINIMUM | 1.56% MINIMUM |
| 2.08% - DESIRABLE | 2.08% - DESIRABLE |
| 2.50% - MAXIMUM | 3.00% - MAXIMUM |
- B. SUPERELEVATION RATE
- S.E. RATE SHOWN ON PLANS OR SE RATE EXISTING IN FIELD, WHICHEVER IS GREATER.
- C. SUPERELEVATION TRANSITION LENGTH (LENGTH FROM FLAT POINT TO FULL SE)
- | | RATE OF CHANGE | CORRESPONDING DIFFERENCE IN GRADE BETWEEN PIVOT POINT AND EDGE OF PAVEMENT |
|-----------|----------------|--|
| MINIMUM | 1:150 | 0.67% |
| DESIRABLE | 1:200 | 0.50% |
| MAXIMUM | 1:300 | 0.33% |
- LENGTH SHALL BE SET TO AVOID CREATING A FLAT GUTTER GRADE ON LOW SIDE AND TO AVOID FLAT CROSS SLOPES AT OR NEAR THE LOW POINT OF VERTICAL CURVES.
- D. POSITIONING OF SUPERELEVATION TRANSITION LENGTH ON SIMPLE CURVES
- 50% OF TRANSITION INSIDE CURVE - MAXIMUM
 - 33% OF TRANSITION INSIDE CURVE - DESIRABLE
 - 20% OF TRANSITION INSIDE CURVE - MINIMUM
- NOTE: CROWN WIPE-OUT SHALL BE AT THE SAME RATE AS THE SE TRANSITION.
- E. SMOOTHING OF BREAKS IN EDGE PROFILE AT BEGIN AND END OF TRANSITION SHALL BE ACCOMPLISHED BY VERTICAL CURVE WITH A MINIMUM LENGTH (IN FEET) EQUAL TO THE SPEED DESIGN (IN MPH).



TEMPORARY DETOUR PAVEMENT
 STA 372+51 TO 374+95 RT SOUTH CALHOUN BYPASS
 STA 29+00 TO 36+40 RT OAK GROVE RD

- (B) RECYCLED ASPH CONC 19 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME (220 LBS/SY)
- (T) RECYCLED ASPH CONC 19 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME (330 LBS/SY)
- (Q) GR AGGR BASE CRS, 8 INCH, INCL MATL
- (K) LIME STABILIZATION



GUARDRAIL LOCATION DETAIL FOR URBAN SHOULDER
 SEE GA STD 4391 FOR ADDITIONAL DETAILS

REVISION DATES

STATE OF GEORGIA
 DEPARTMENT OF TRANSPORTATION
 OFFICE: PROGRAM DELIVERY
TYPICAL SECTIONS

STP00-00MS-00(007)
 SOUTH CALHOUN BYPASS

DRAWING No.
05-011

HL Heath & Lineback Engineers
 INCORPORATED
 2390 CANTON ROAD, BUILDING 200
 MARIETTA, GEORGIA 30066-5393

NO SCALE