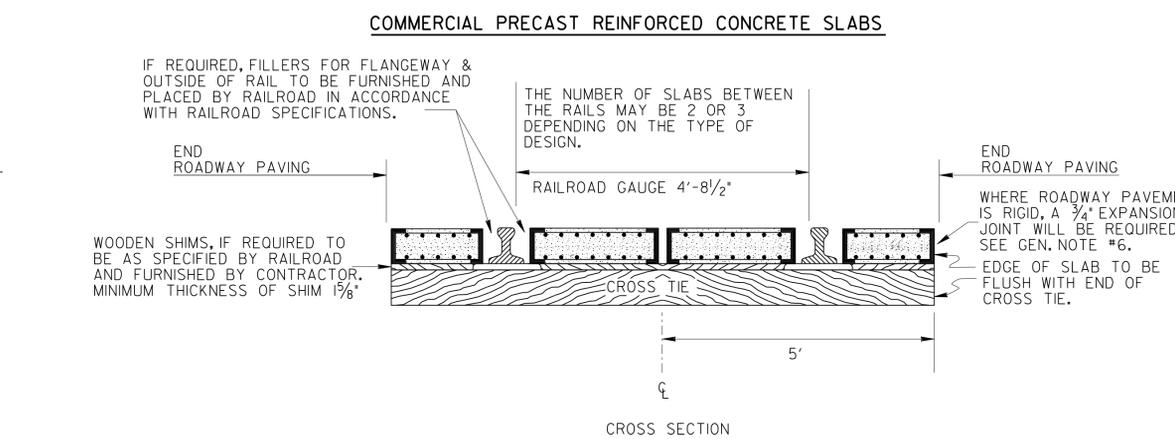
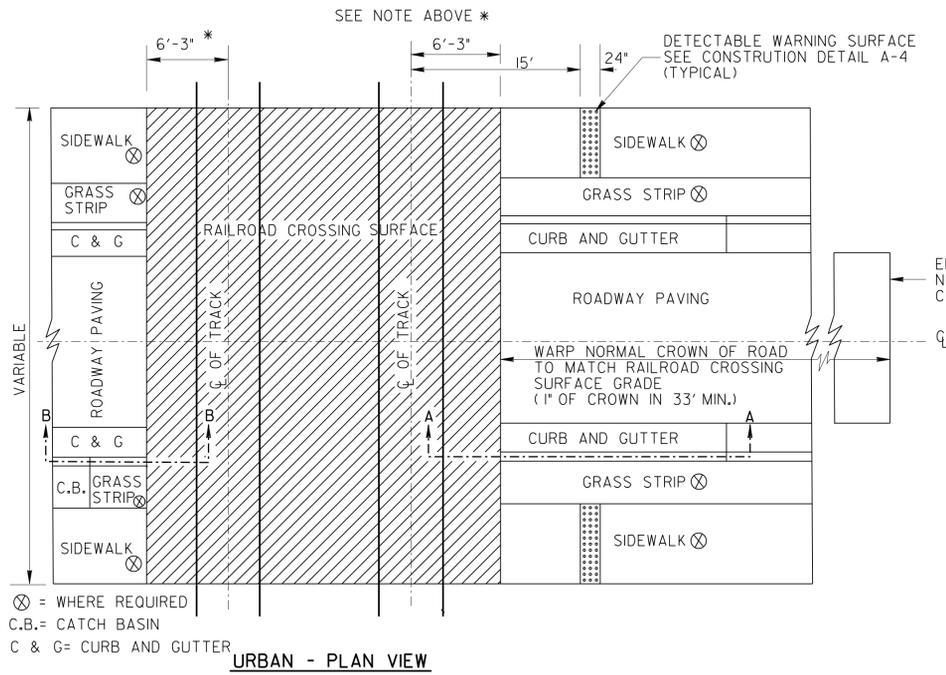
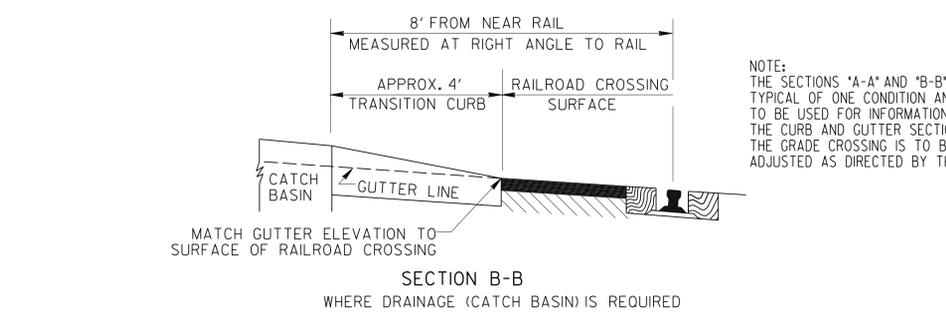
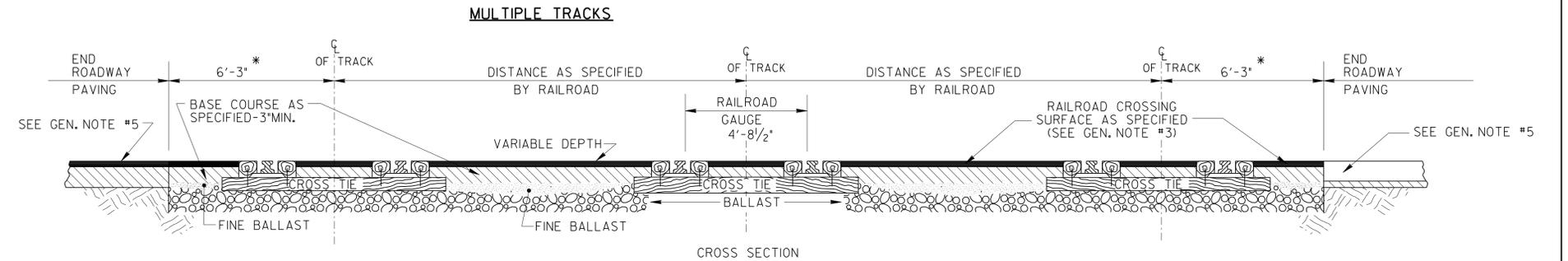
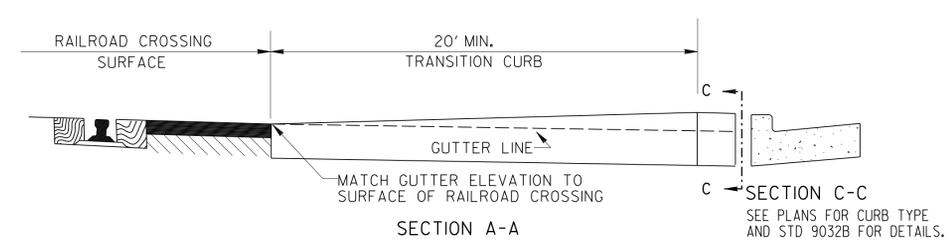


- GENERAL NOTES:**
- SPECIFICATIONS: GEORGIA STANDARDS, CURRENT EDITION & SUPPLEMENTS THERETO. A) SEE SECTION 232 FOR GRADE CROSSING, PRECAST CONCRETE BID ITEM PAYMENT IF APPLICABLE. B) SEE SECTION 232 FOR BOLTED CROSSING, INSTALLED BID ITEM PAYMENT IF APPLICABLE.
 - THE RAILROAD IS TO BE NOTIFIED BEFORE ANY WORK IS DONE BY GA. D.O.T. MAINTENANCE FORCES OR GA. D.O.T. CONTRACTORS WITHIN 25' OF C.L. OF TRACK. A RAILROAD REPRESENTATIVE IS TO BE PRESENT TO SUPERVISE THE WORK BEING DONE.
 - RAILROAD CROSSING SURFACE, BALLAST, TIES, RUNNING RAILS, GUARD TIMBERS AND FILLERS, IF REQUIRED FOR FLANGWAY AND OUTSIDE OF RAIL, ARE TO BE FURNISHED AND PLACED BY RAILROAD WITH COSTS, ELIGIBLE FOR REIMBURSEMENT BEING BILLED TO THE STATE UNDER FORCE ACCOUNT AGREEMENTS, UNLESS OTHERWISE SPECIFIED.
 - ONLY AT THE RAILROAD'S REQUEST, THE GA. D.O.T. CONTRACTOR TO FURNISH ALL WORK AND MATERIALS NECESSARY FOR CONSTRUCTING THE PAVING COURSES OF THE CROSSING. THE COST OF FLAGGING SERVICE REQUIRED BY THE RAILROADS FOR ANY CONSTRUCTION ACTIVITY AT GRADE CROSSINGS SHALL BE A COST TO THE CONTRACTOR. THIS COST SHALL BE BORNE BY THE CONTRACTOR, WHETHER THE FLAGGING SERVICE IS PERFORMED BY THE CONTRACTOR'S PERSONNEL OR, IF REQUIRED BY THE RAILROADS, TO BE DONE BY THE RAILROAD PERSONNEL.
 - ONLY ONE SIDE OF THE ROADWAY PAVING, ADJACENT TO THE RAILROAD CROSSING SURFACE, CAN BE RIGID. A MINIMUM OF 15' OF FLEXIBLE PAVEMENT IS REQUIRED ADJACENT TO THE OTHER SIDE OF THE RAILROAD CROSSING SURFACE.
 - REFER TO AASHTO, POLICY ON GEOMETRIC DESIGN OF HIGHWAYS AND STREETS, FOR ADDITIONAL DESIGN GUIDANCE ON RAILROAD-HIGHWAY GRADE CROSSINGS (SECTION 9J2).



- GENERAL SPECIFICATIONS FOR COMMERCIAL PRECAST REINFORCED CONCRETE SLAB FOR RAILWAY CROSSING**
- THE PLACING OF PRECAST REINFORCED CONCRETE SLABS WILL BE PERMITTED WHERE APPROVED BY THE RAILROAD & THE STATE UTILITIES ENGINEER OR DESIGNEE.
 - SLABS TO BE OF THE TYPE THAT CAN BE ANCHORED TO TIES AND REMOVED.
 - IF SLABS ARE OVER MULTIPLE TRACKS, THE INTERTRACK SLABS ARE TO BE PLACED ON COMPACTED EARTH.
 - THE DEPTH OF THE SLABS SHALL BE SUCH THAT THE TOP SURFACE OF THE CROSSING WILL BE IN THE SAME PLANE AS THE TOP OF THE RAIL. SHIMS WILL BE USED IF REQUIRED.
 - ALL EXPOSED EDGES OF SLAB WILL BE ARMORED.
 - PROTECTION OF THE END SLABS AGAINST DRAGGING BRAKE RIGGING WILL BE PROVIDED.
 - IN GENERAL, THE DESIGN SLABS WILL BE IN ACCORDANCE WITH (AREMA) AMERICAN RAILWAY ENGINEERING AND MAINTENANCE-OF-WAY ASSOCIATION SPECIFICATIONS.
 - THE TYPE OF COMMERCIAL PRECAST CONCRETE SELECTED BY THE CONTRACTOR MUST BE APPROVED BY THE RAILROAD & THE STATE UTILITIES ENGINEER OR DESIGNEE.
 - THE COMMERCIAL PRECAST CROSS SECTION IS A TYPICAL REPRESENTATION OF ONE TYPE OF MANUFACTURED SURFACE AND IS SHOWN FOR INFORMATION ONLY. DIMENSIONS MAY VARY WITH DIFFERENT DESIGNS.
- ALTERNATE:**
COMMERCIAL PREFABRICATED OR MANUFACTURED SECTIONS OTHER THAN REINFORCED CONCRETE SLABS MAY BE USED PROVIDED THE DESIGNS AND MATERIALS ARE APPROVED BY THE GA. D.O.T. AND BY THE RAILROAD, WITH THE ABOVE GENERAL SPECIFICATIONS APPLYING.



DATE		DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA	
REVISION		STANDARD RAILROAD GRADE CROSSING PAVING (& SIGNAL CONDUIT LOCATIONS) NO SCALE REV. & REDR. SEPT. 2016	
BY	DES. (SUBMITTED)	TRA. (APPROVED)	NUMBER 9021A
	DRW. <i>B. A. Stitt</i>	CHK. <i>Margaret B. Pirelo</i>	STATE DESIGN POLICY ENGINEER CHIEF ENGINEER