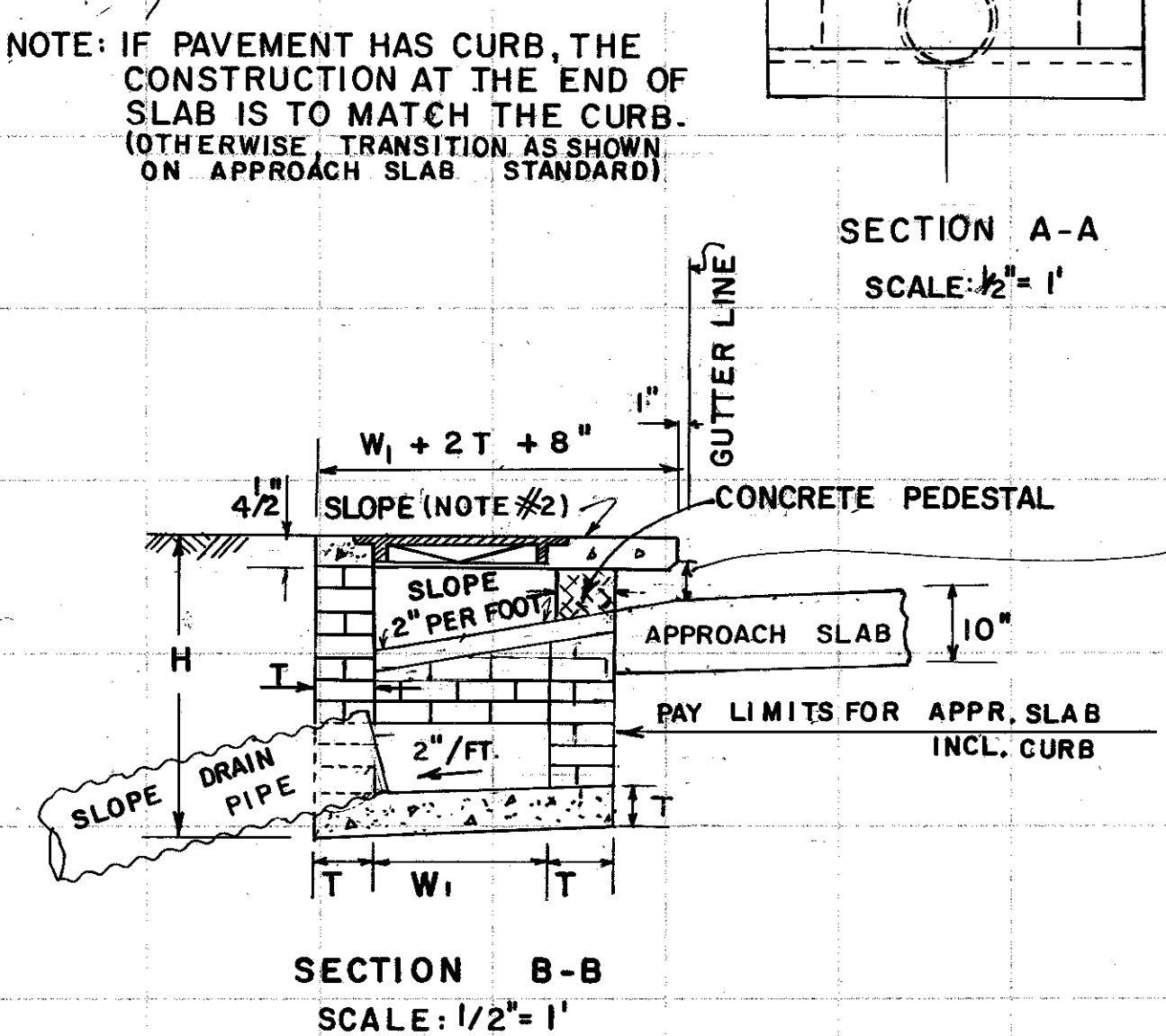
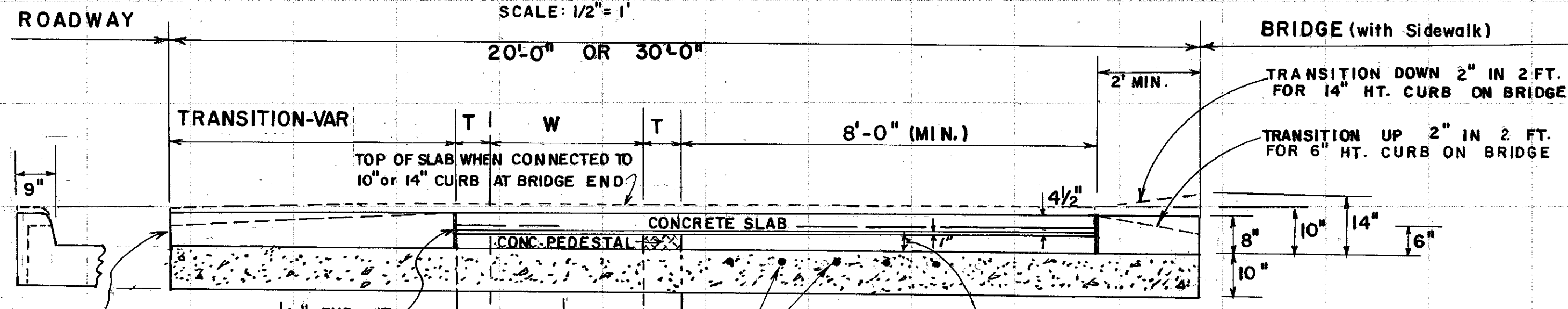
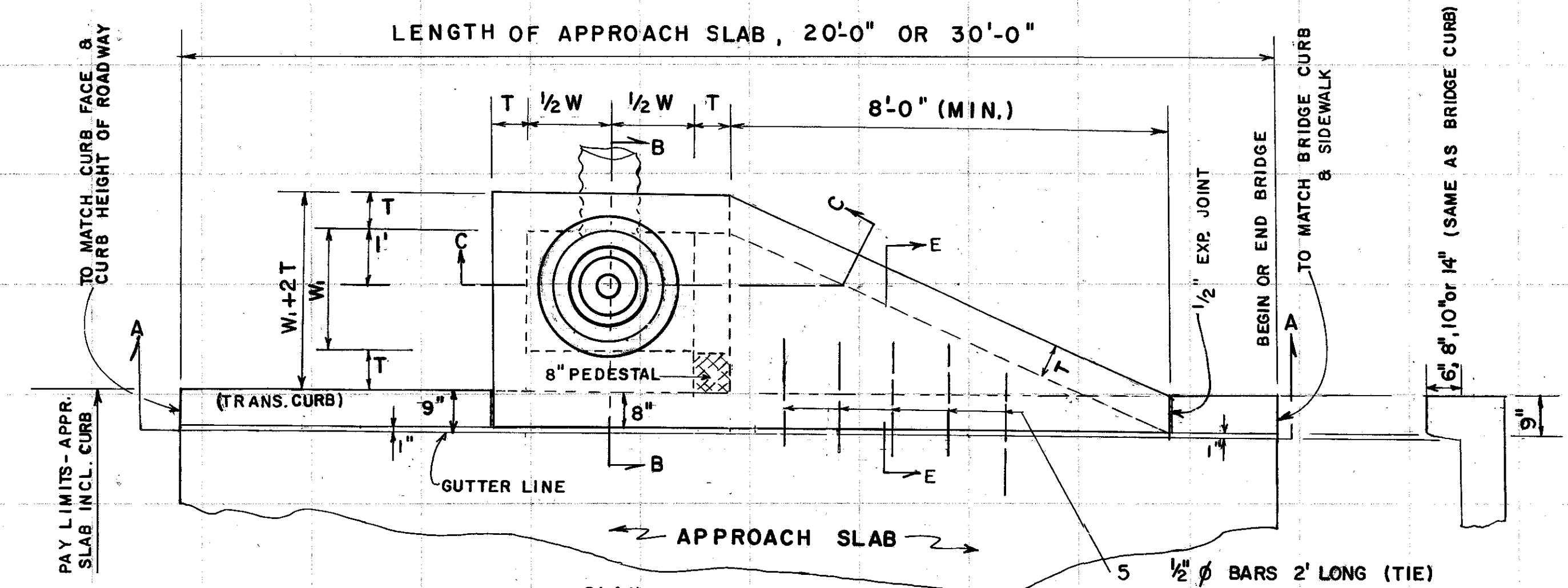
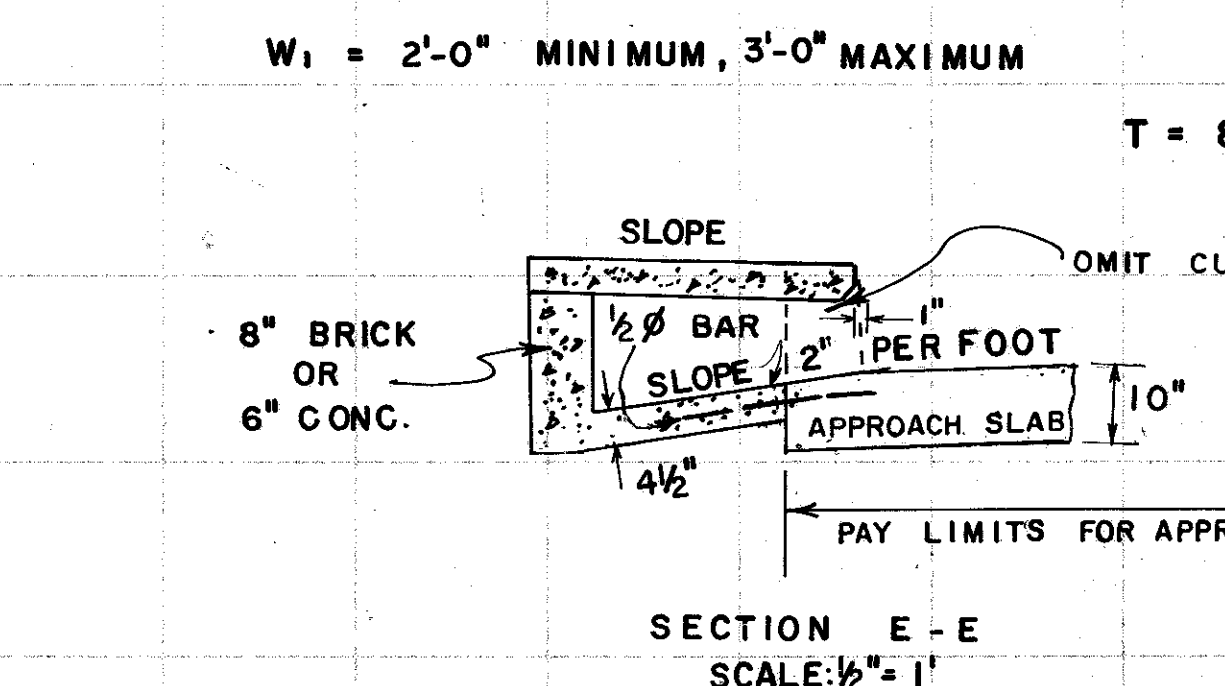
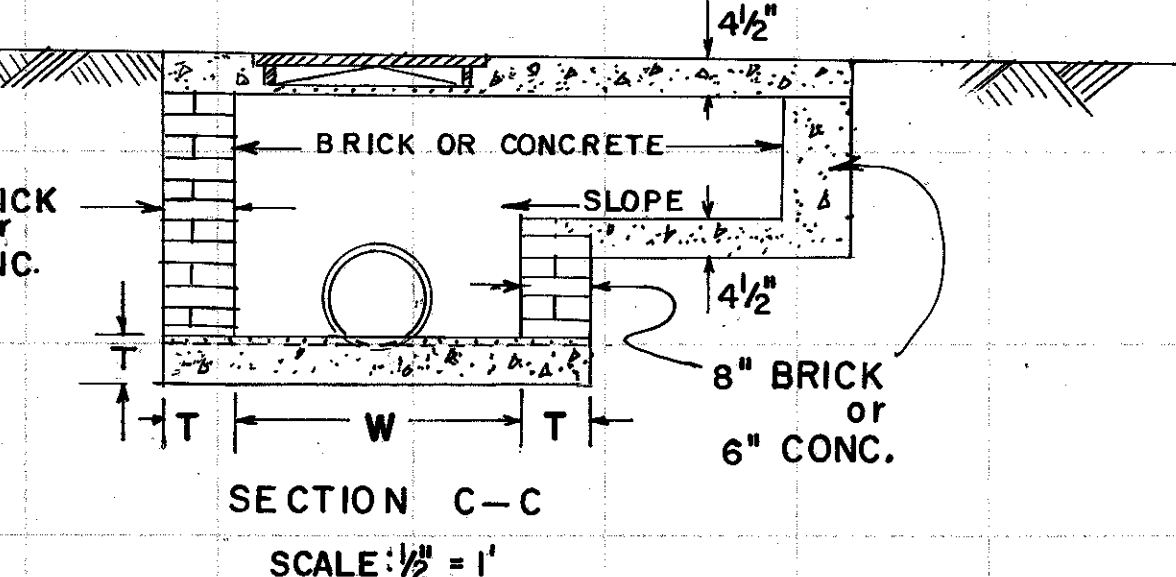


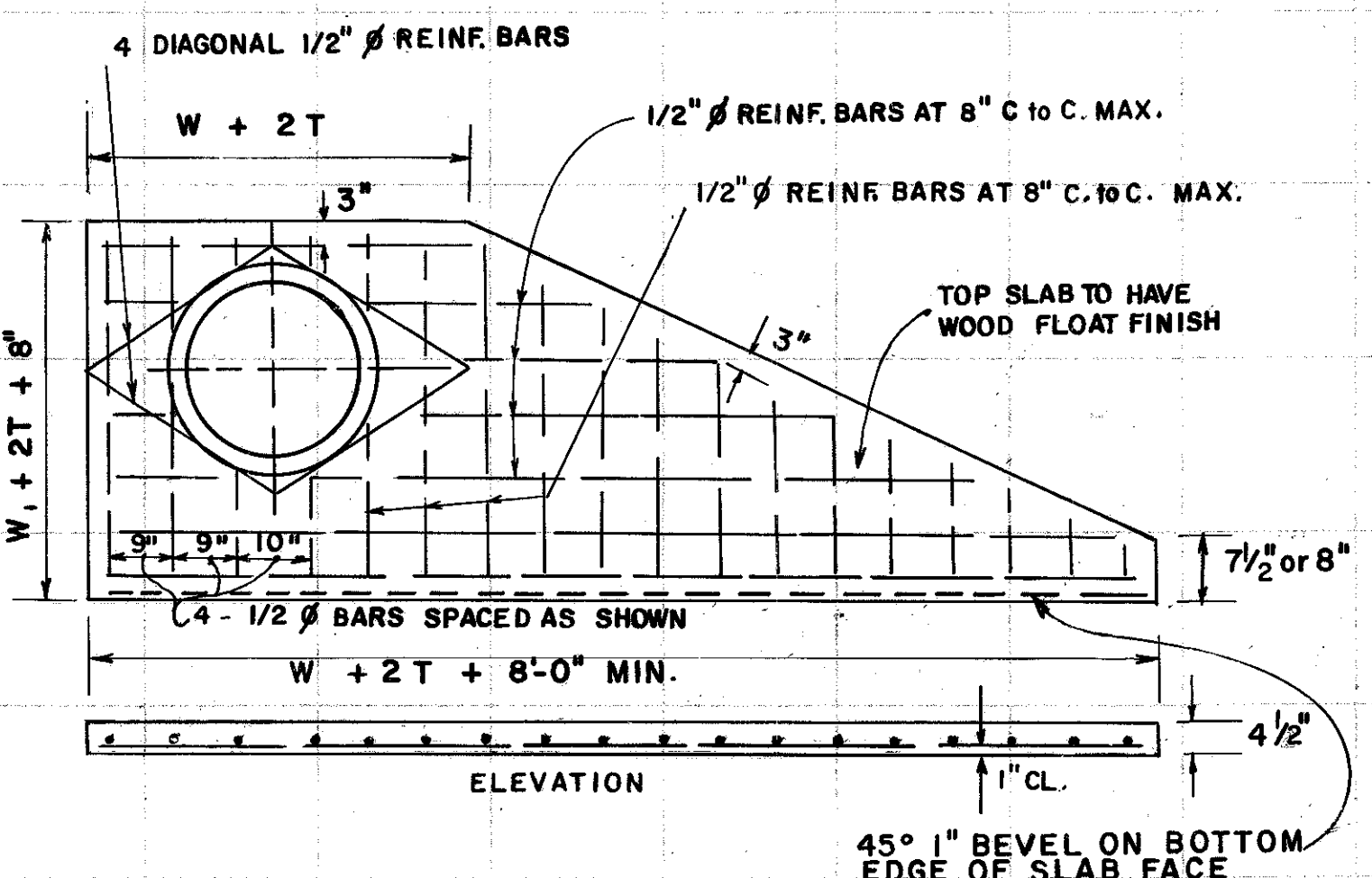
CATCH BASIN (BUILT IN PLACE) ADJACENT TO APPROACH SLAB



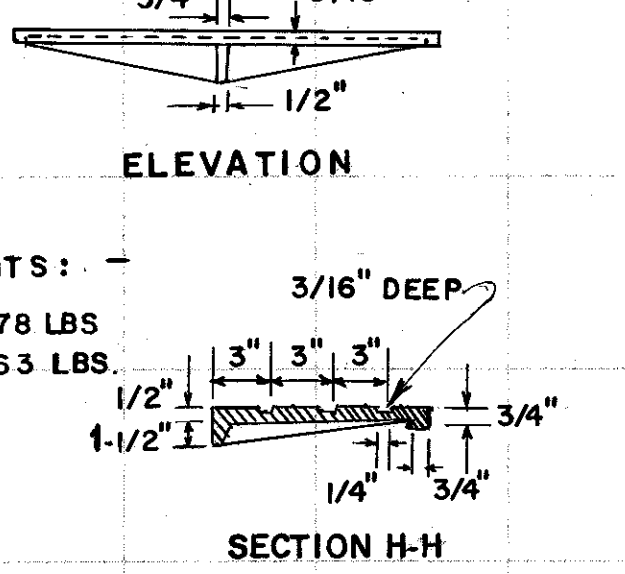
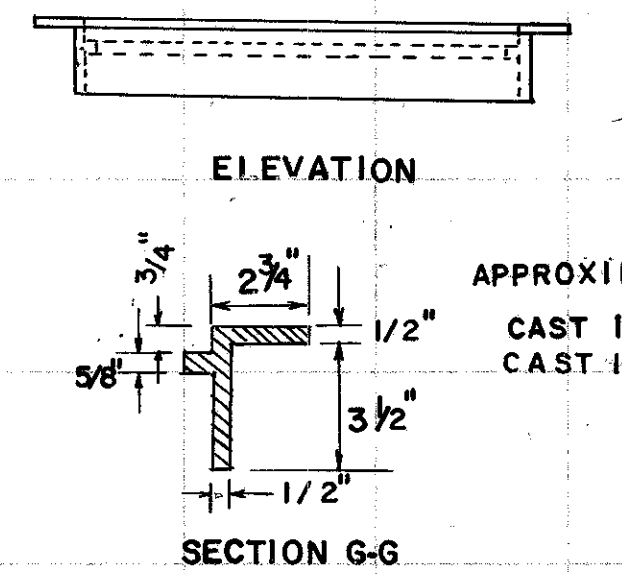
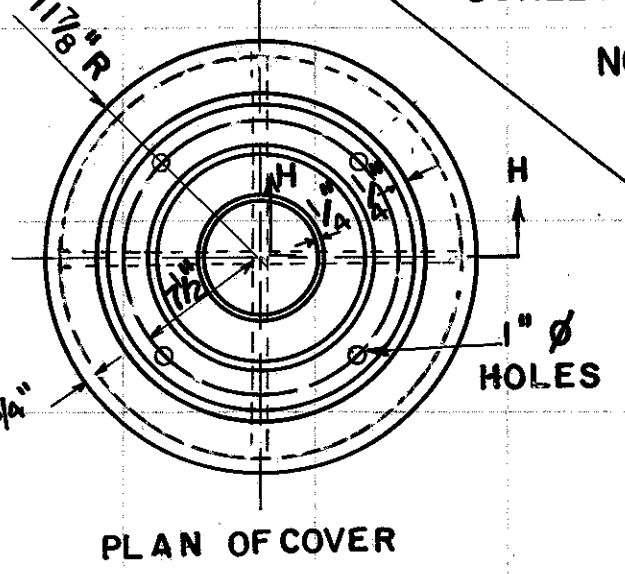
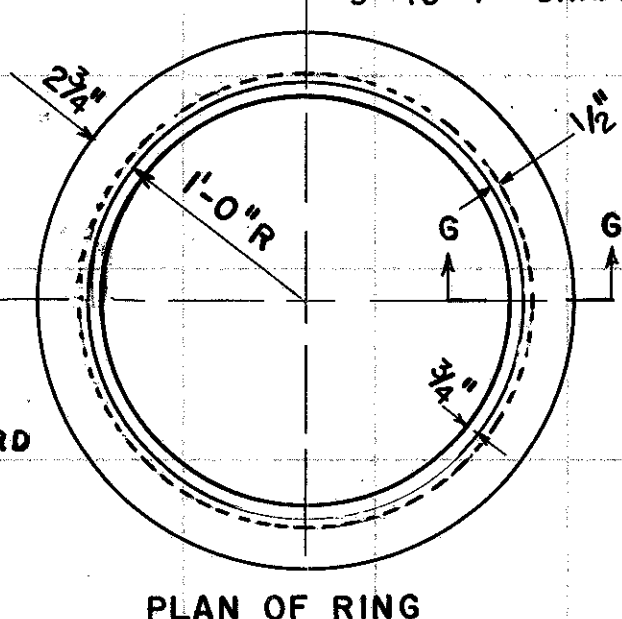
PIPE SIZE	MIN. H
10"	3'-4"
12"	3'-6"
15"	3'-9"
18"	4'-0"



REINFORCED CONCRETE TOP SLAB

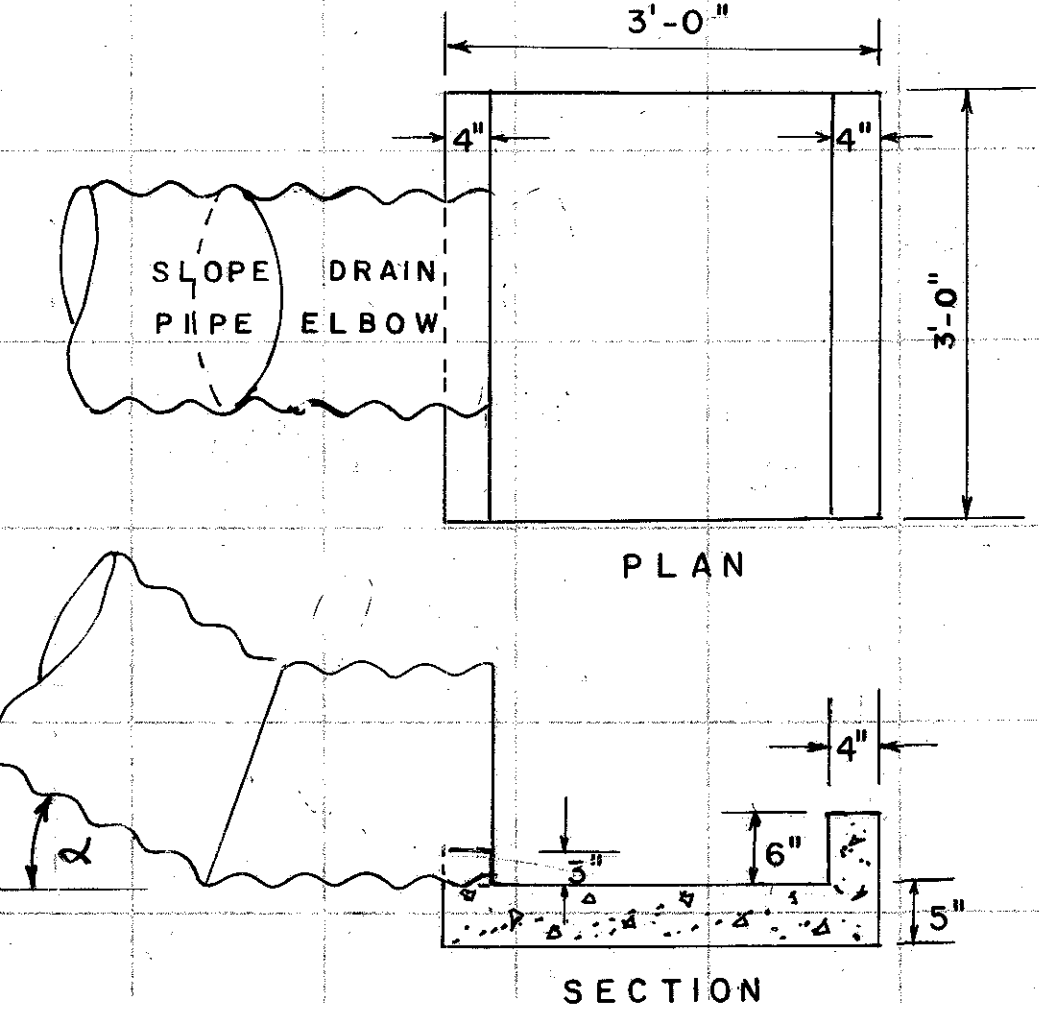


CASTING DETAILS



APPROXIMATE WEIGHTS:  
CAST IRON RING: 78 LBS.  
CAST IRON COVER: 63 LBS.

APRON AT OUTLET OF SLOPE DRAIN PIPE

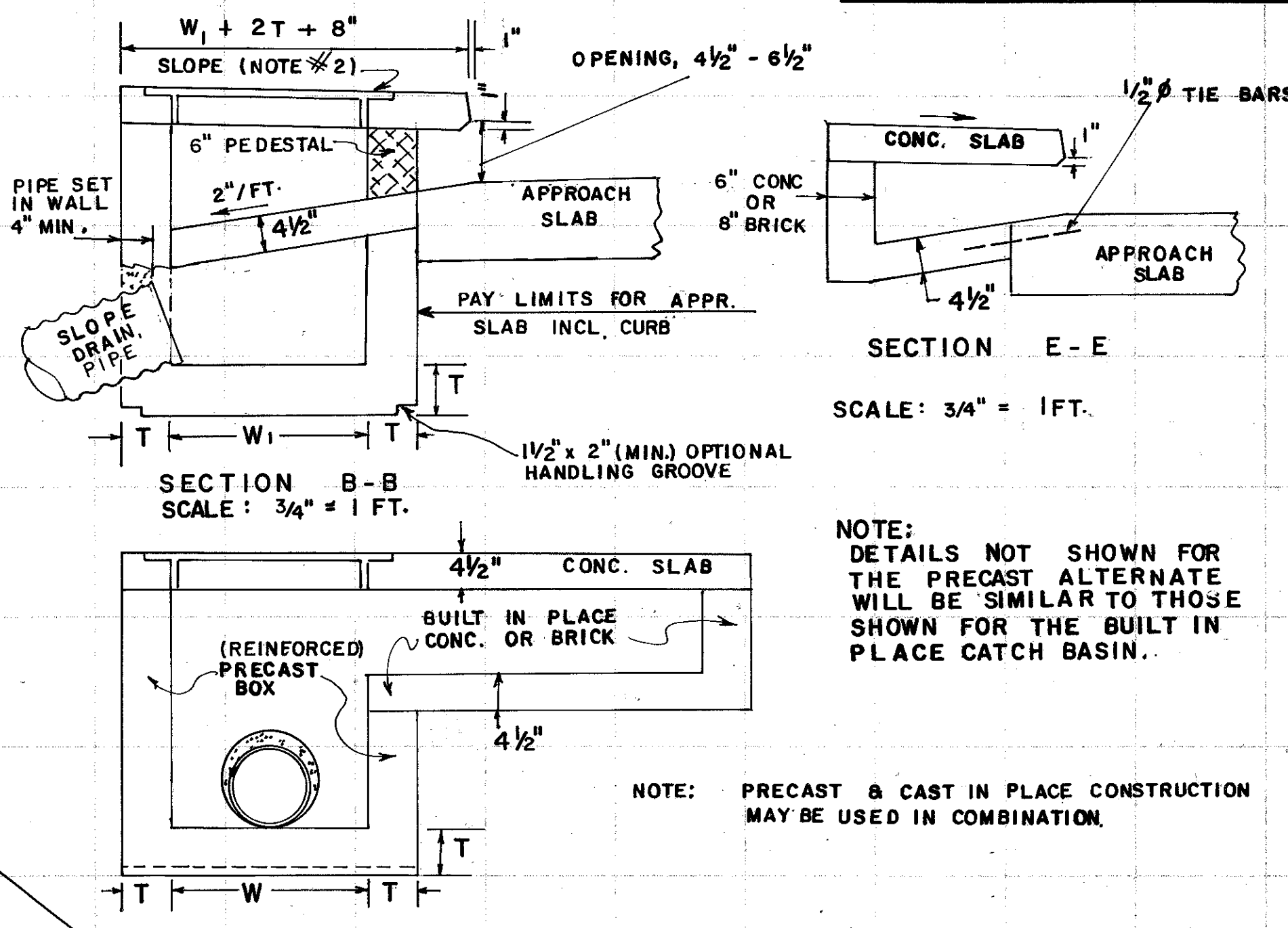


QUANTITY:  
0.17 CU YDS CL. "B" CONC.  
OR CL. "A" CONC.

ALTERNATE:  
APRON MAY BE CONSTRUCTED TO APPROXIMATELY THE SAME DIMENSIONS FROM SAND CEMENT BAG RIP RAP, STONE GROUT RIP RAP, OR MORTAR RUBBLE MASONRY.

ELBOW ANGLES:  
FILL SLOPE: 30°  
FLATTER THAN 2:1: 15°

(PRECAST ALTERNATE)



GENERAL NOTES:

- (a) NORMAL USE OF THIS STANDARD IS WHERE CURB OR CURB AND GUTTER IS ADJACENT TO ROADWAY WITH CURB AND/OR SIDEWALK ACROSS BRIDGE. SEE STD. 9017J FOR SPILLWAYS (RURAL SECTION TYP.).
- (b) THE LOCATION OF GUARDRAIL SHOULD BE DETERMINED BEFORE CATCH BASIN IS SPECIFIED. IF TOP SLAB CONFLICTS WITH GUARDRAIL POSTS, DRAINAGE SHOULD BE CARRIED ACROSS THE APPROACH SLAB TO A ROADWAY DROP INLET.
- CATCH BASIN TOP SLAB SHALL BE SLOPED 1/4" PER FT. TOWARDS THE APPROACH SLAB OR SHALL BE SLOPED TO MATCH ADJACENT SURFACES OR TO GIVE THE DESIRED TRANSITION BETWEEN THE ROADWAY AND BRIDGE SIDEWALK GRADES AND CURB HEIGHTS.
- SEE SEPARATE STANDARD (9017K OR 9017L TYPICAL) FOR APPROACH SLAB DETAILS.
- A 1/2" EXPANSION JOINT SHALL BE REQUIRED AT ALL POINTS WHERE A CATCH BASIN INTERCEPTS CONCRETE SIDEWALK.
- SPECIFICATIONS: GEORGIA STANDARD, CURRENT EDITION, AND SUPPLEMENTS THERETO.

DEPARTMENT OF TRANSPORTATION  
STATE OF GEORGIA

STANDARD  
CATCH BASIN  
(PRECAST OR BUILT IN PLACE)  
ADJACENT TO APPROACH SLAB

SCALE AS SHOWN  
REV. & REDR. AUGUST, 1983  
REV. RMU (SUBMITTED) *David G. Hardy*  
REDR. RMU (STATE ROAD & AIRPORT DESIGN ENGR.)  
TRA. GME (APPROVED) *H. H. Rouse*  
CHK. RKC (STATE HIGHWAY ENGINEER)

NUMBER  
9017C