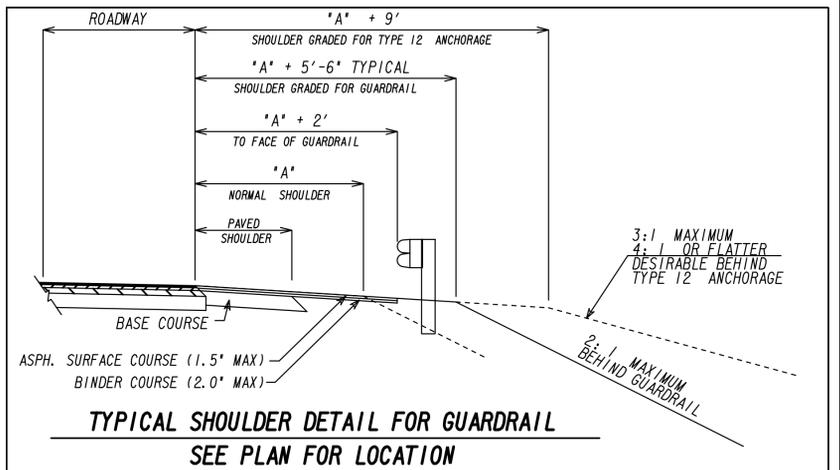
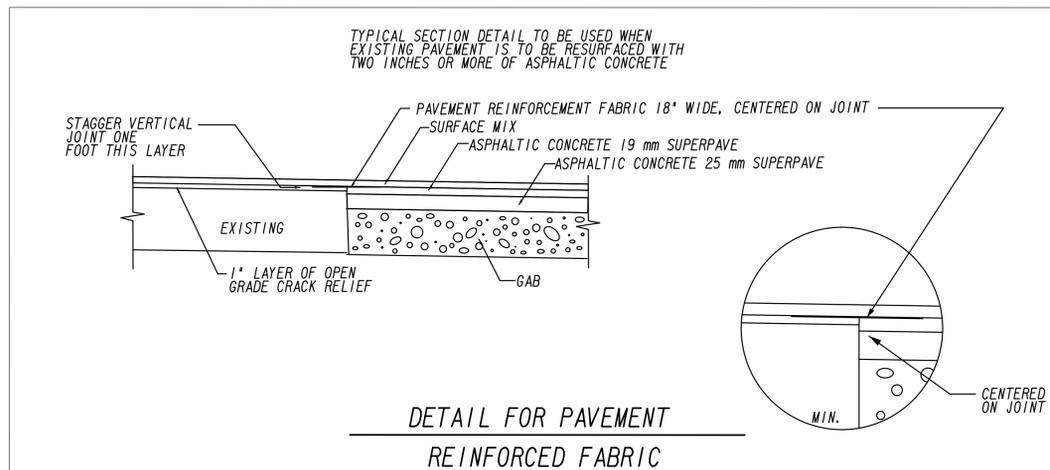
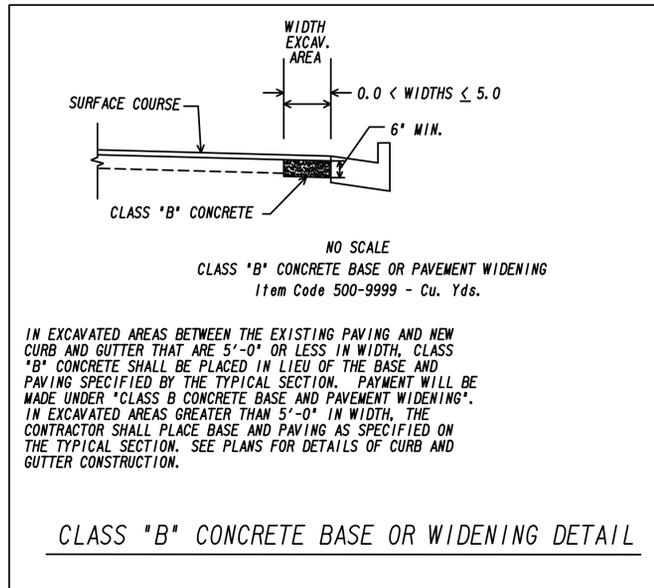


TYPICAL SECTION 6
 I-95 SB OFF RAMP
 STA. 500+00.00 TO STA. 503+95.25

PAVEMENT SECTION	
(A)	RECYCLED ASPH CONC 12.5 MM SUPERPAVE, GP 2 ONLY, INCL POLYMER-MODIFIED BITUM MATL & H LIME (220 BS/SY)
(B)	RECYCLED ASPH CONC 19 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME (220 LBS/SY)
(C)	RECYCLED ASPH CONC 25 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME (660 LBS/SY)
(D)	GRADED AGGREGATE BASE CRS, INCL MATL, 12 INCH
(E)	ASPHALTIC CONCRETE OPEN GRADED CRACK RELIEF INTERLAYER, GP 2 ONLY, INCL BITUM MATL & H LIME (110 LBS/SY)
(F)	MILL ASPH CONC, PVMT, VARIABLE DEPTH
(G)	CONC CURB & GUTTER, 8 IN X 30 IN, TP 2
(H)	CONCRETE HEADER CURB, 6 IN, TP 2
(I)	CONC. SLOPE PAV, 4 IN
(J)	PAVEMENT EDGE TREATMENT - SEE GDOT DETAIL P-7
(K)	RECYCLED ASPH CONC LEVELING, INCL BITUM MATL & H LIME



ALLOWABLE RANGES TABLE

FOR THIS PROJECT, CROSS SLOPES THAT ARE ADJUSTED TO "BEST FIT" EXISTING PAVEMENT SLOPES ARE SUBJECT TO THE FOLLOWING LIMITS:

A. NORMAL CROWN

SECTION WITH GRADES 0.5% OR GREATER	SECTION WITH GRADES LESS THAN 0.5%
0.0150 FT/FT - MINIMUM	0.0156 FT/FT - MINIMUM
0.0208 FT/FT - DESIRABLE	0.0208 FT/FT - DESIRABLE
0.0250 FT/FT - MAXIMUM	0.0300 FT/FT - MAXIMUM

B. SUPERELEVATION RATE

S.E. RATE SHOWN ON PLANS OR SE RATE EXISTING IN FIELD, WHICHEVER IS GREATER.

C. SUPERELEVATION TRANSITION LENGTH (LENGTH FROM FLAT POINT TO FULL SE)

RATE OF CHANGE	CORRESPONDING DIFFERENCE IN GRADE BETWEEN PIVOT POINT AND EDGE OF PAVEMENT
MINIMUM 1:150	0.67%
DESIRABLE 1:200	0.50%
MAXIMUM 1:300	0.33%

LENGTH SHALL BE SET TO AVOID CREATING A FLAT GUTTER GRADE ON LOW SIDE AND TO AVOID FLAT CROSS SLOPES AT OR NEAR THE LOW POINT OF VERTICAL CURVES.

D. POSITIONING OF SUPERELEVATION TRANSITION LENGTH ON SIMPLE CURVES

50% OF TRANSITION INSIDE CURVE - MAXIMUM
 33% OF TRANSITION INSIDE CURVE - DESIRABLE
 20% OF TRANSITION INSIDE CURVE - MINIMUM

NOTE: CROWN WIPE-OUT SHALL BE AT THE SAME RATE AS THE SE TRANSITION.

E. SMOOTHING OF BREAKS IN EDGE PROFILE AT BEGIN AND END OF TRANSITION SHALL BE ACCOMPLISHED BY VERTICAL CURVE WITH A MINIMUM LENGTH (IN FEET) EQUAL TO THE SPEED DESIGN (IN MPH).

REVISION DATES	STATE OF GEORGIA DEPARTMENT OF TRANSPORTATION OFFICE: INNOVATIVE DELIVERY
	TYPICAL SECTIONS
	I-95 AT SR 21 DIVERGING DIAMOND INTERCHANGE PROJECT
	DRAWING No. 05-0004

W
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NOT TO SCALE