

DAMAGE DESCRIPTION (FROM COLLISION REPORT)

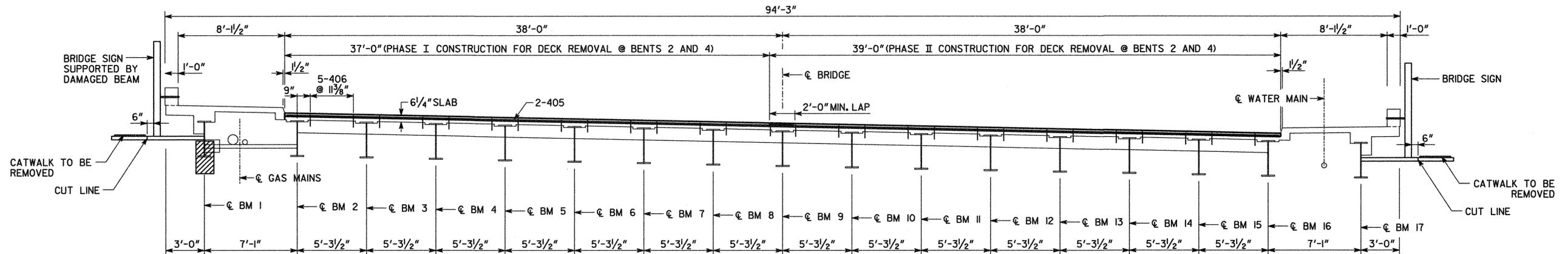
BEAM 1, SPAN 2 - BOTTOM FLANGE IS BENT UP AND DOWN WITH TEAR IN BOTTOM FLANGE. WEB IS BENT WITH UP TO 3" SWEEP.



- INDICATING DAMAGED BEAM AND STEEL DIAPHRAGM REQUIRING REPAIR

NOTE:

1. TYPICAL SECTION SHOWN IS FROM EXISTING DRAWING.

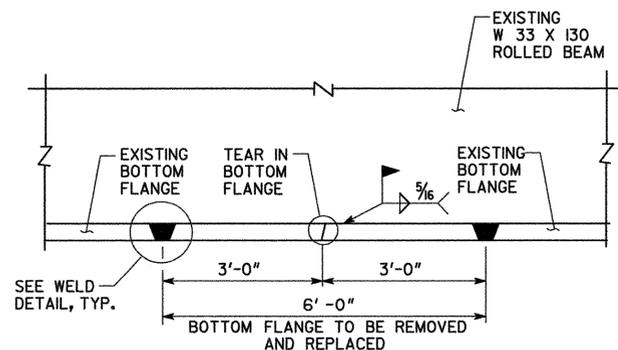


DECK SECTION

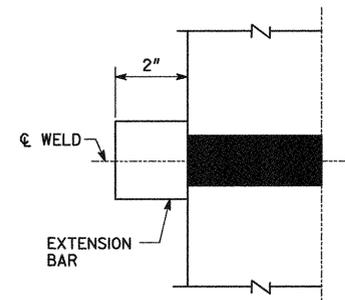
LOOKING AHEAD
NOT TO SCALE

SEQUENCE OF STEEL BEAM REPAIR

1. INSTALL PORTABLE MESSAGE SIGN IN ACCORDANCE WITH GEORGIA SPECIFICATION SECTION 632 AND AS DIRECTED BY ENGINEER. MESSAGE ON PORTABLE CHANGEABLE SIGN SHALL REPLACE MESSAGE ON BRIDGE SUPPORT SIGN.
2. REMOVE BRIDGE SIGN CONNECTED TO DAMAGED BEAM.
3. SUPPORT BEAM & UTILITIES PRIOR TO BOTTOM FLANGE REMOVAL.
4. HEAT STRAIGHTEN DAMAGED BEAM.
5. REPLACE BOTTOM FLANGE OF DAMAGED BEAM. SEE DETAIL THIS SHEET.
6. REINSTALL BRIDGE SIGN AS PER GDOT CONSTRUCTION DETAIL T-7.



BOTTOM FLANGE REPLACEMENT DETAIL



SECTION B-B

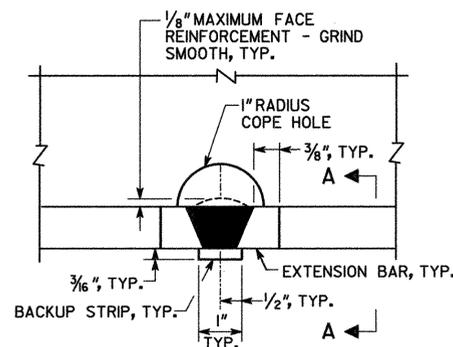
MINIMUM PREHEAT AND INTERPASS TEMPERATURE

THICKNESS OF THICKEST PART AT POINT OF WELDING

THICKNESS	GRADE 36	GRADE 50
ZERO TO 3/4" INCLUSIVE	NONE	NONE
OVER 3/4" TO 1/2" INCLUSIVE	150° F	50° F
OVER 1/2" TO 2/2" INCLUSIVE	225° F	150° F
OVER 2/2" TO 4" INCLUSIVE	300° F	225° F

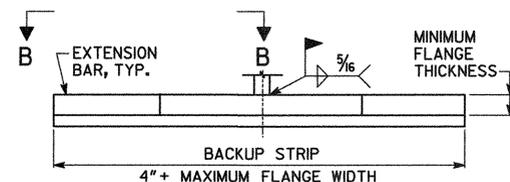
NOTES:

1. WORK SHALL BE PROTECTED FROM MOISTURE DURING WELDING AND UNTIL PARTS WELDED HAVE COOLED TO ATMOSPHERIC TEMPERATURE.
2. USE TWIN ARC TECHNIQUE ON BOTTOM FLANGE WELDS.
3. AFTER WELDING, REMOVE THE BACKUP STRIPS AND EXTENSION BARS. WELDS SHALL BE GROUND SMOOTH. GRINDING SHALL BE DONE PARALLEL TO THE LENGTH OF THE BEAM TO A SMOOTH FINISH. CHIPPING WILL NOT BE ALLOWED. ANY VISUAL IMPERFECTIONS SHALL BE REPAIRED BY WELDING AND GRINDING.
4. STRUCTURAL STEEL FOR EXISTING ROLLED BEAMS IS A-373.



WELD DETAIL

SHADED AREAS REPRESENT V-GROOVE WELDS



SECTION A-A

BRIDGE NO. 1



2835 BRANDYWINE ROAD
SUITE 400
ATLANTA, GEORGIA 30341
(770) 936-8650

GEORGIA
DEPARTMENT OF TRANSPORTATION
ENGINEERING DIVISION-OFFICE OF BRIDGE DESIGN

STEEL BEAM REPAIR DETAIL
BRIDGE REHAB ON
CS 1002 (LEE STREET) OVER SR 402 (I-20)
FULTON COUNTY CSSTP-M004-00(055)

SCALE: NO SCALE APRIL 2010

DRAWING NO. 4 OF 7	DESIGNED AS DRAWN AS	CHECKED TQT DESIGN GROUP MAINTENANCE	REVIEWED CBB APPROVED MLC
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REVISIONS	DATE
TOT	5-26-10
TOT	8-13-10
TOT	DELETED NOTE
TOT	MODIFIED DECK SECTION