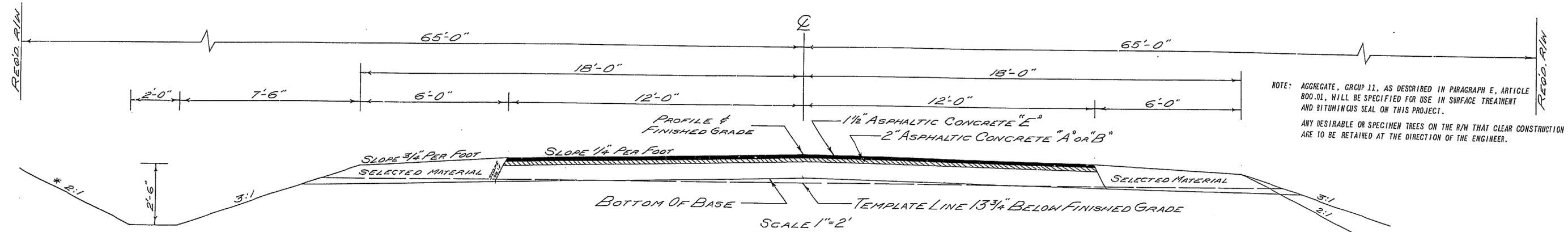


REVISED 5-2-67  
REVISED 6-6-67

# TYPICAL GRADING & PAVING SECTION



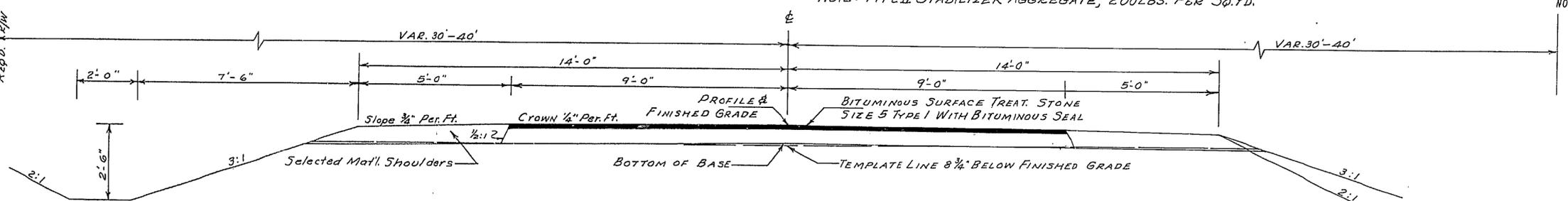
NOTE: AGGREGATE, GROUP 11, AS DESCRIBED IN PARAGRAPH E, ARTICLE 800.01, WILL BE SPECIFIED FOR USE IN SURFACE TREATMENT AND BITUMINOUS SEAL ON THIS PROJECT.  
ANY DESIRABLE OR SPECIMEN TREES ON THE R/W THAT CLEAR CONSTRUCTION ARE TO BE RETAINED AT THE DIRECTION OF THE ENGINEER.

\*BACKSLOPES MAY BE FLATTENED, AS DIRECTED BY THE ENGINEER, TO ELIMINATE BORROW.

8" x 24'-7 1/2" GRADED AGGREGATE BASE COURSE (PREMIXED)  
(CEMENT STABILIZED) 24'-2 1/2" x 2" ASPHALTIC CONCRETE  
"A" OR "B" AND 24'-0 3/4" x 1 1/2" ASPHALTIC CONCRETE "E"  
AND SELECTED MATERIAL SHOULDERS.

NOTE: TYPE II STABILIZER AGGREGATE, 200 LBS. PER SQ. YD.

ROADWAY TO BE SUPERELEVATED AND WIDENED IN ACCORDANCE WITH GA. STD. 9028-H.  
POINT OF ROTATION LOCATED AT CENTERLINE FOR EARTHWORK COMPUTATIONS, BUT WHERE NECESSARY MAY BE SHIFTED TO THE OUTSIDE OR INSIDE EDGE OF PAVEMENT TO BETTER FIT TOPOGRAPHY OR IMPROVE DRAINAGE.



6" x 18'-4" GRADED AGGREGATE BASE COURSE (PREMIXED)  
(CEMENT STABILIZED) - 18'-0 1/2" BITUMINOUS SURFACE TREAT.  
STONE SIZE 5 TYPE I WITH SEAL & SELECTED MAT'L. SHOULDERS

THIS SECTION APPLIES FROM STA. 0+12.8 TO STA. 2+50 RELOC. CO. ROAD RT. STA. 26+87.5  
STA. 107+12.1 TO STA. 114+00 RELOC. S-0846 LT. STA. 107+00

NOTE: TYPE II STABILIZER AGGREGATE, 150 LBS. PER SQ. YD.

NOTE: IN GENERAL, THE GOVERNING SLOPE ON FILL SHALL BE 3:1, EXCEPTION TO BE MADE ON FILL OVER 5' FOR A LONG DISTANCE WHERE A 5:1 SLOPE IS NOT ECONOMICAL, IN WHICH CASE A SLOPE OF 2:1 WILL BE USED.

ROADWAY TO BE GRADED FLAT AND TO THE SAME ELEVATION AS THE BOTTOM OF BASE COURSE AT A POINT MIDWAY BETWEEN THE CENTERLINE OF PAVEMENT AND SHOULDER AND AS DENOTED BY THE BROKEN LINE.

AFTER GRADING HAS BEEN COMPLETED TO THE TEMPLATE LINE THE ROADWAY WILL BE CROWNED TO THE SAME SLOPE AS THE FINISHED PAVEMENT. CROWN SLOPE TO PIVOT ABOUT A POINT HALFWAY BETWEEN CENTERLINE OF PAVEMENT AND SHOULDER POINT AND AT AN ELEVATION IDENTICAL WITH BOTTOM OF BASE. BASE MATERIAL WILL BE PLACED TO THE COMPACTED PLAN DEPTH AND WIDTH OF ROADWAY, THEN SHAPED TO THE REQUIRED SECTION.

DATA SHOWN ON PLANS REGARDING CLASSIFICATION, LOCATION, EXTENT OF SOIL OR ROCK AT STRUCTURE SITES, OR IN PITS, ARE BASED ON FIELD INVESTIGATIONS, AND ARE BELIEVED TO BE INDICATIVE OF ACTUAL CONDITIONS. THESE DATA ARE SHOWN AS INFORMATION ONLY, NOT GUARANTEED, AND DO NOT BIND THE STATE HIGHWAY DEPARTMENT IN ANY WAY.

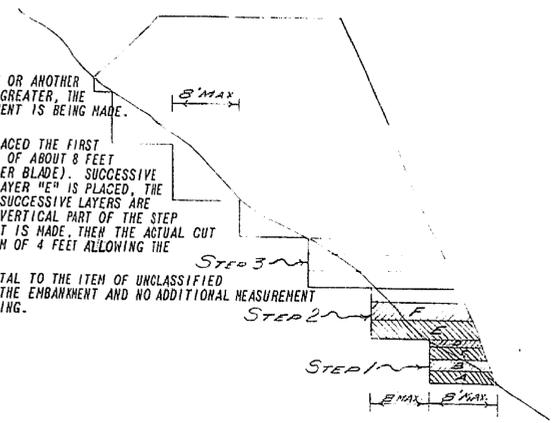
SEE GA. STD. 9031-H FOR DETAILS:

- SPRIGGING
- TURNOUTS
- METHOD FOR ROADBED AND SUBGRADE TREATMENT
- FLARES

THIS PROJECT IS CLASSIFIED AS A GRADING AND PAVING PROJECT.  
NO PAYMENT WILL BE MADE FOR FINISHING AND DRESSING.

## BENCHING DETAILS

- WHERE THE EMBANKMENT FOUNDATION IS A HILLSIDE OR ANOTHER EXISTING EMBANKMENT HAVING A SLOPE 1 ON 3 OR GREATER, THE FOUNDATION MUST BE BENCHING WHILE THE EMBANKMENT IS BEING MADE. (SEE DIAGRAM ABOVE.)
- THE DIAGRAM SHOWS THAT BEFORE LAYER "A" IS PLACED THE FIRST STEP IS CUT INTO THE SLOPE A MAXIMUM DISTANCE OF ABOUT 8 FEET (ABOUT 3/4 THE WIDTH OF THE USUAL D-8 BULLDOZER BLADE). SUCCESSIVE LAYERS B, C, AND D ARE THEN PLACED. BEFORE LAYER "E" IS PLACED, THE SECOND STEP IS CUT 8 FEET INTO THE SLOPE AND SUCCESSIVE LAYERS ARE AGAIN PLACED. IF IT IS ANTICIPATED THAT THE VERTICAL PART OF THE STEP WILL EXCEED 4 FEET IF AN 8 FOOT HORIZONTAL CUT IS MADE, THEN THE ACTUAL CUT STOPS WHEN THE VERTICAL PART REACHES A MAXIMUM OF 4 FEET ALLOWING THE HORIZONTAL DISTANCE TO VARY.
- THE PROCESS OF BENCHING IS CONSIDERED INCIDENTAL TO THE ITEM OF UNCLASSIFIED EXCAVATION AND BORROW IN THE CONSTRUCTION OF THE EMBANKMENT AND NO ADDITIONAL MEASUREMENT OF QUANTITY OR PAYMENT WILL BE MADE FOR BENCHING.



## LENGTH OF PROJECT

STATION - STATION	LINEAL FEET			EQUALITIES AND EXCEPTIONS	
	ROADWAY		BRIDGE		
	S-1599 (3)	S-0846 SPUR (1)	S-0846 SPUR (1)		
10 + 11.4	26 + 87.5				
26 + 87.5	83 + 50.33				
83 + 50.33	89 + 73.67		23.33 *	* DOL. 10 x 10" REINFC. CONC. ON CURB	
89 + 73.67	113 + 15.7				
TOTAL	1676.1	8604.86	23.33		
	S-1599 (3)		S-0846 SPUR (1)		
	LINE. FT.	MILES	LINE. FT.	MILES	TOTAL
NET LENGTH OF ROADWAY	1676.1	0.317	8604.86	1.629	1.946
NET LENGTH OF BRIDGES	0.0	0.000	23.33	0.004	0.004
NET LENGTH OF PROJECT	1676.1	0.317	8628.19	1.633	1.950
NET LENGTH OF EXCEPTIONS	0.0	0.000	0.00	0.000	0.000
GROSS LENGTH OF PROJECT	1676.1	0.317	8628.19	1.633	1.950

THE CONTRACTOR'S ATTENTION IS DIRECTED TO SEC. 4.05A, 5.10 AND 7.06 OF THE GENERAL SPECIFICATIONS IN REGARD TO MAINTAINING TRAFFIC.