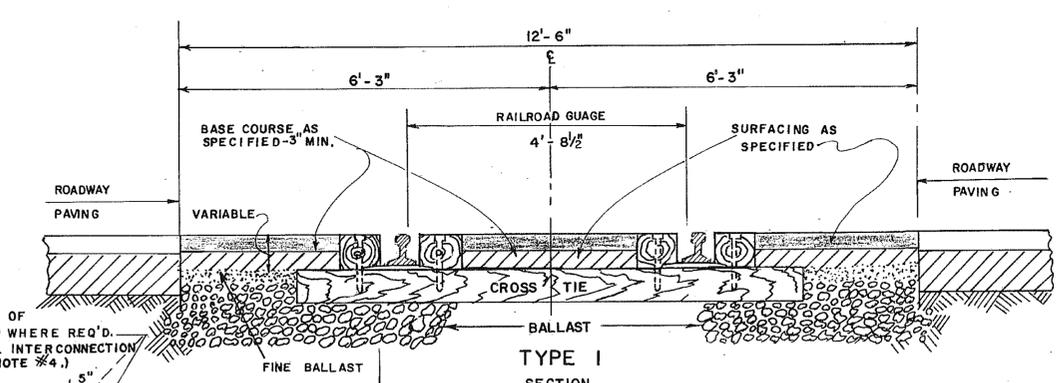
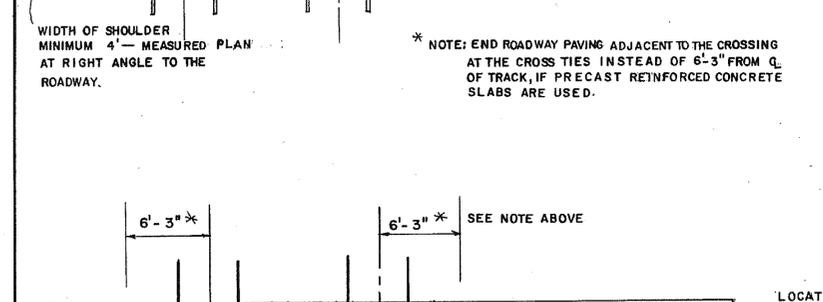
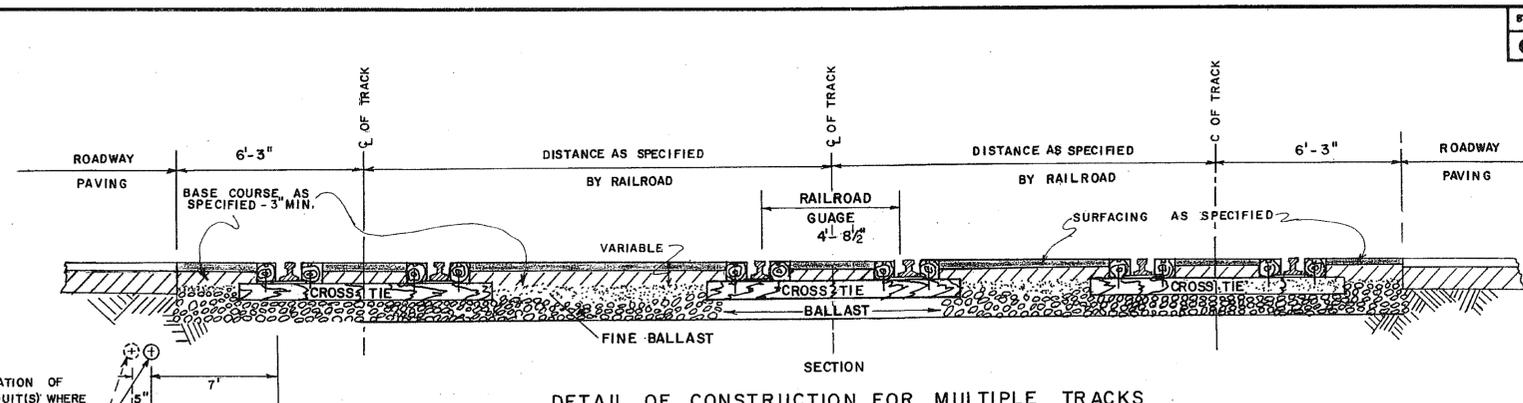
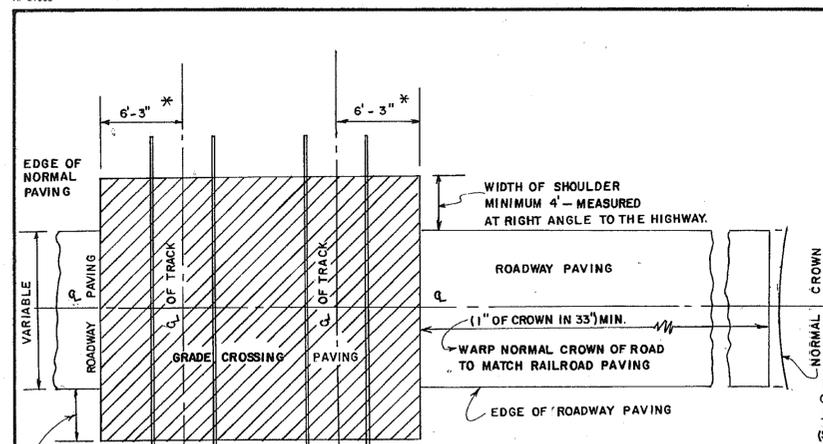
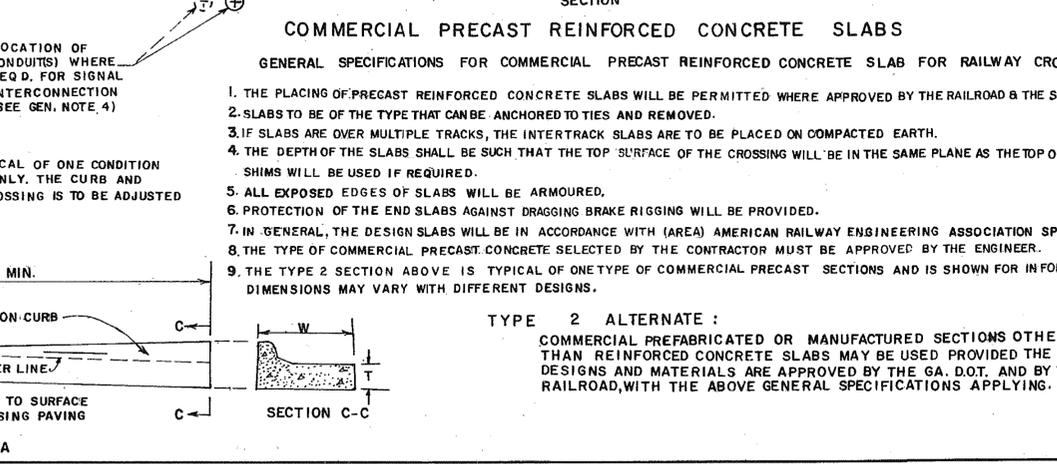
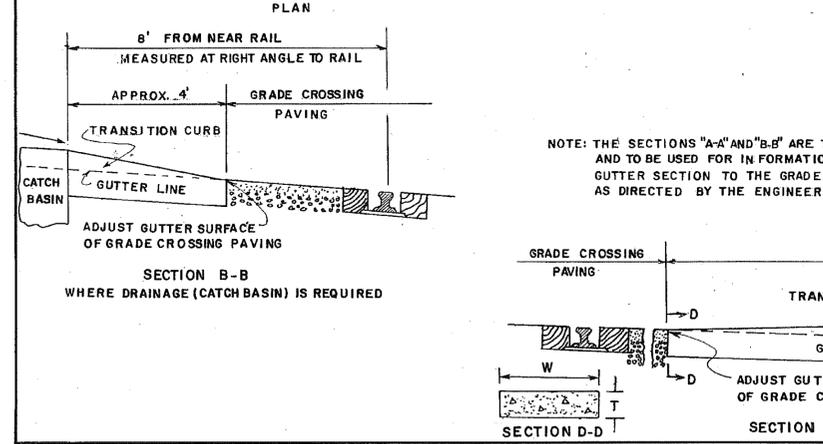
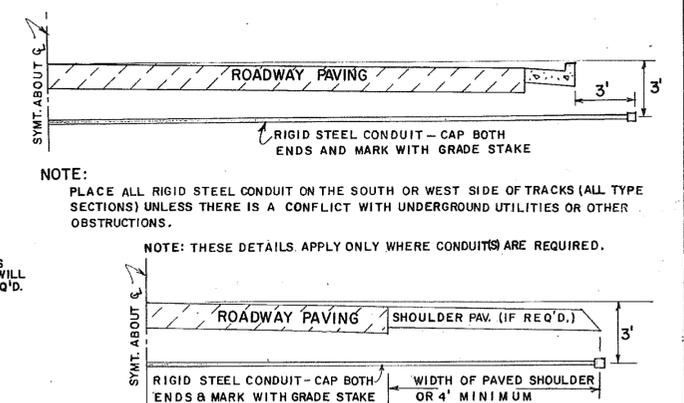
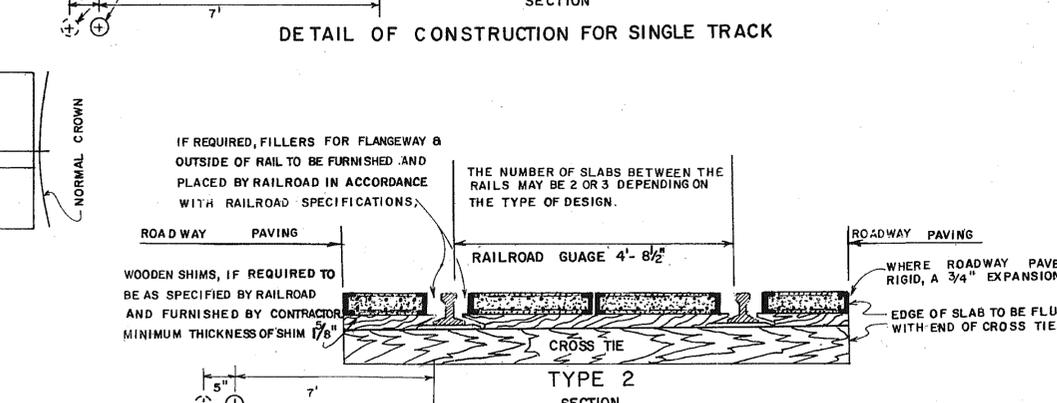
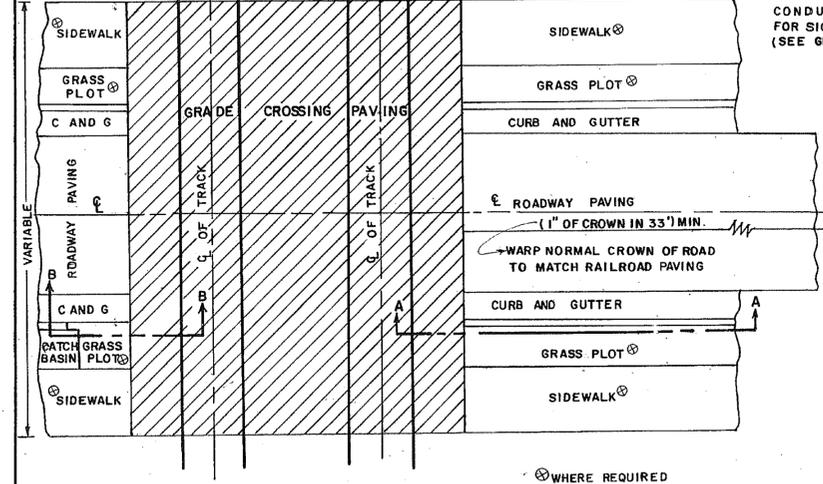


STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA	BRMLB-9004-001(008)	578	701



- GENERAL NOTES:
1. THE RAILROAD IS TO BE NOTIFIED BEFORE ANY WORK IS DONE BY GA. DOT. MAINTENANCE FORCES OR GA. DOT. CONTRACTORS BETWEEN THE RAILS AND WITHIN 8' OF CL OF TRACK ON TANGENTS AND 10' OF CL OF TRACK ON CURVES. A RAILROAD REPRESENTATIVE IS TO BE PRESENT TO SUPERVISE THE WORK BEING DONE AND TO PROTECT THE FORCES WORKING, FROM RAILWAY TRAFFIC.
 2. BALLAST TIES, RUNNING RAILS, GUARD TIMBERS AND FILLERS, IF REQUIRED FOR FLANGEWAY AND OUTSIDE OF RAIL, ARE TO BE FURNISHED AND PLACED BY RAILROAD WITH COSTS ELIGIBLE FOR REIMBURSEMENT; BEING BILLED TO THE STATE UNDER FORCE ACCOUNT AGREEMENTS.
 3. THE GA. DOT. CONTRACTOR TO FURNISH ALL WORK AND MATERIALS NECESSARY FOR CONSTRUCTING THE PAVING COURSES OF THE CROSSING. THE COST OF FLAGGING SERVICE REQUIRED BY THE RAILROADS FOR ANY CONSTRUCTION ACTIVITY AT GRADE CROSSINGS SHALL BE A COST TO THE CONTRACTOR. THIS COST SHALL BE BORNE BY THE CONTRACTOR, WHETHER THE FLAGGING SERVICE IS PERFORMED BY THE CONTRACTOR'S PERSONNEL OR, IF REQUIRED BY THE RAILROADS, TO BE DONE BY THE RAILROAD PERSONNEL.
 4. WHERE CONDUIT(S) ARE REQUIRED FOR SIGNAL INTERCONNECTIONS, TWO 4" RIGID STEEL CONDUITS ARE FOR THE SEABOARD COAST LINE, LOUISVILLE & NASHVILLE SYSTEM, GEORGIA RR, ATLANTA & WEST POINT RR, AND SOUTHERN RR. ONE 4" RIGID STEEL CONDUIT IS SHOWN (7 FT. FROM THE OUTSIDE TRACK) FOR OTHER RAILROADS.



DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA

STANDARD
RAILROAD GRADE CROSSING
PAVING
(& SIGNAL CONDUIT LOCATIONS)

NO SCALE REV. & RECD. SEPT, 1982

REV. & RMU RECD.	(SUBMITTED) <i>Floyd E. Hardy</i>	NUMBER
TRA. GME	STATE ROAD & AIRPORT DESIGN ENGR.	9021 A
CHK. RKC	(APPROVED) <i>Thomas D. Hardy</i>	
	STATE HIGHWAY ENGINEER	

COMMERCIAL PRECAST REINFORCED CONCRETE SLABS

GENERAL SPECIFICATIONS FOR COMMERCIAL PRECAST REINFORCED CONCRETE SLAB FOR RAILWAY CROSSING

1. THE PLACING OF PRECAST REINFORCED CONCRETE SLABS WILL BE PERMITTED WHERE APPROVED BY THE RAILROAD & THE STATE HIGHWAY ENGINEER.
2. SLABS TO BE OF THE TYPE THAT CAN BE ANCHORED TO TIES AND REMOVED.
3. IF SLABS ARE OVER MULTIPLE TRACKS, THE INTERTRACK SLABS ARE TO BE PLACED ON COMPACTED EARTH.
4. THE DEPTH OF THE SLABS SHALL BE SUCH THAT THE TOP SURFACE OF THE CROSSING WILL BE IN THE SAME PLANE AS THE TOP OF THE RAIL. SHIMS WILL BE USED IF REQUIRED.
5. ALL EXPOSED EDGES OF SLABS WILL BE ARMoured.
6. PROTECTION OF THE END SLABS AGAINST DRAGGING BRAKE RIGGING WILL BE PROVIDED.
7. IN GENERAL, THE DESIGN SLABS WILL BE IN ACCORDANCE WITH (AREA) AMERICAN RAILWAY ENGINEERING ASSOCIATION SPECIFICATIONS.
8. THE TYPE OF COMMERCIAL PRECAST CONCRETE SELECTED BY THE CONTRACTOR MUST BE APPROVED BY THE ENGINEER.
9. THE TYPE 2 SECTION ABOVE IS TYPICAL OF ONE TYPE OF COMMERCIAL PRECAST SECTIONS AND IS SHOWN FOR INFORMATION ONLY. DIMENSIONS MAY VARY WITH DIFFERENT DESIGNS.

TYPE 2 ALTERNATE:
COMMERCIAL PREFABRICATED OR MANUFACTURED SECTIONS OTHER THAN REINFORCED CONCRETE SLABS MAY BE USED PROVIDED THE DESIGNS AND MATERIALS ARE APPROVED BY THE GA. DOT. AND BY THE RAILROAD, WITH THE ABOVE GENERAL SPECIFICATIONS APPLYING.