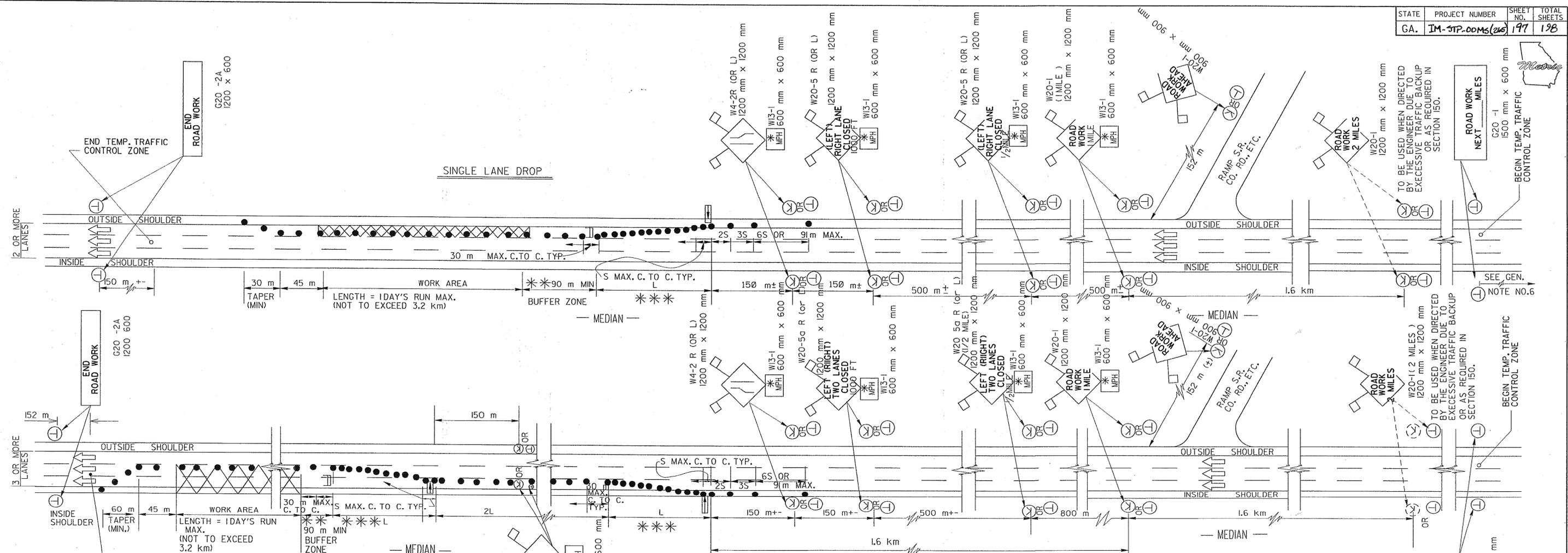


STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.	IM-5TP-00MS(26)	197	198



POSTED SPEED PRIOR TO WORK	S			
	S	2S	3S	6S OR 90 m MAX
MPH	(m)	(m)	(m)	(m)
25	7.5	15	22.5	45
30	9	18	27	54
35	10.5	21	31.5	63
40	12	24	36	72
45	13.5	27	40.5	81
50	15	30	45	90
55	16.5	33	49.5	90

(NOT APPLICABLE FOR CONTINUOUS TRAFFIC CONTROL - SEE NOTE 4)
(SEE FORMULAE FOR OTHER CONDITIONS)

STANDARD LEGEND

- 450 mm X 450 mm FLOURESCENT RED/ORANGE OR ORANGE/RED WARNING FLAG
- STRIPED DRUM
- △ TRAFFIC CONE - 28" MIN. (DAYTIME USE ONLY)
- ▬ TYPE II BARRICADE (OPTIONAL)
- ⇄ SEQUENTIAL OR FLASHING ARROW
- ⊕ TEMPORARY POST MOUNTED SIGN (OFF SHOULDER) --FOR LONG TERM LANE CLOSURE SUCH AS STATIONARY OPERATIONS, BRIDGE WIDENING PROJECTS ETC. - NO FLAGS REQUIRED (7' MOUNT HEIGHT)
- Ⓚ PORTABLE MOUNTED SIGN (ON SHOULDER) --FOR SHORT TERM LANE CLOSURE SUCH AS MOVING OPERATIONS, RESURFACING PROJECTS, ETC. - FLAGS REQUIRED (1' TO 3' MOUNT HEIGHT)
- ▨ WORK AREA

$L = 0.625 S \times W$ FOR SPEEDS OF 70 km/h OR GREATER;
 $L = W \times S^2 / 155$ FOR SPEEDS OF 65 km/h OR LESS
 WHERE:
 L = MIN. LENGTH OF TAPER (IN METERS)
 S = POSTED SPEED (IN km/hr)
 W = WIDTH OF OFFSET (IN METERS)

NOTE: MIN. TAPER LENGTH = 200 m. FOR 90 Km/h

GENERAL NOTES:

1. ALL TRAFFIC CONTROL DEVICES SHALL BE MADE AND ERECTED IN ACCORDANCE WITH THE DETAILS SHOWN ON THE PLANS; THE MUTCD; THE GEORGIA STANDARD SPECIFICATIONS, AND/OR SPECIAL PROVISIONS.
2. ALL TRAFFIC CONTROL DEVICES SHALL BE AS SHOWN OR AS DIRECTED BY THE ENGINEER. ADDITIONAL DEVICES MAY BE REQUIRED AS DIRECTED BY THE ENGINEER.
3. ALL PORTABLE MOUNTED SIGNS SHALL BE MOUNTED SO THAT THE BOTTOM OF THE SIGN WILL BE BETWEEN 300 mm AND 900 mm ABOVE THE PAVEMENT EDGE. ALL TEMPORARY POST MOUNTED SIGNS SHALL BE MOUNTED SO THAT THE BOTTOM OF THE SIGN SHALL BE 2.1 m MINIMUM ABOVE PAVEMENT EDGE OR 2.1 m MINIMUM, IF ROADWAY CONDITIONS DICTATE.
4. WHEN THE CONSTRUCTION AREA HAS ENTRANCE/EXIT RAMP OR INTERSECTIONS, WORK WILL BE PERFORMED IN SUCH A MANNER TO PERMIT TRAFFIC TO OPERATE WITH THE LEAST AMOUNT OF INCONVENIENCE AS POSSIBLE. ADDITIONAL CHANNELIZATION AND SIGNING SHALL BE INSTALLED, AS REQUIRED, TO ALLOW TRAFFIC TO REMAIN AS OPERATIONAL AS POSSIBLE. WHEN ENTRANCE/EXIT RAMP/INTERSECTIONS ARE INOPERABLE, FLAGGERS WILL BE UTILIZED TO CONTROL AND PROHIBIT MOVEMENT INTO THE PROJECT AT THAT POINT UNTIL CONSTRUCTION HAS CLEARED THE RESTRICTION SUFFICIENT TO RETURN TO OPERATIONAL STATUS.
5. FOR NIGHTTIME OPERATIONS, ALL DRUMS IN TAPERS ONLY SHALL HAVE 250x250 YELLOW PANELS WITH TYPE V REFLECTIVE SHEETING. DURING DAYLIGHT HOURS ONLY, CONES (700 mm MIN.) MAY BE USED FOR LONGITUDINAL CHANNELIZATION ONLY.
6. SIGNS SHOWN HERE ARE IN ADDITION TO ALL ADVANCE WARNING SIGNS REQUIRED BY SECTION 150. WARNING FLAGS AND FLASHING LIGHTS ON SIGNS SHALL BE AS REQUIRED BY SECTION 150.
7. A PORTABLE SELF-SUSTAINED SEQUENTIAL OR FLASHING ARROW SIGN SHALL BE USED AT THE BEGINNING OF EACH LANE CLOSURE.
8. WHEN NOT IN USE, PORTABLE SIGNS SHALL BE REMOVED FROM THE TRAVELWAY SO THAT THE MESSAGE IS NOT VISIBLE TO THE MOTORIST. INTERIM SIGNS THAT ARE PERMANENT MOUNTED SHALL BE COVERED WHEN NOT APPLICABLE. SEE SECTION 150.
9. PAYMENT FOR TRAFFIC CONTROL SHALL BE PER SECTION 150.
10. (a) ON PROJECTS WITH LOW OR SOFT SHOULDERS, THE CONTRACTOR SHALL ERECT IMMEDIATELY AHEAD OF CONSTRUCTION OPERATIONS "LOW/SOFT SHOULDER" WARNING SIGNS AT THE PROJECT TERMINI, AT INTERVALS NOT TO EXCEED 1.6 km AND IMMEDIATELY PAST EACH CROSSROAD.
 (b) WHERE THE CONTRACTOR IS NOT RESPONSIBLE FOR SHOULDER CONSTRUCTION, THE DEPARTMENT WILL FURNISH THESE SIGNS FOR THE CONTRACTOR TO PICK UP, TRANSPORT AND ERECT, THE DEPARTMENT WILL LATER REMOVE AND RETAIN THE SIGNS.
11. IF EXISTING ADVANCE WARNING SIGNS (ROAD WORK "W20-1") ARE IN PLACE AND ARE IN CONFLICT WITH THE LANE CLOSURE SIGNS SHOWN, THEY SHALL BE RESET IN ADVANCE OF LANE CLOSURE WITH THE MINIMUM SIGN SPACING REQUIRED BY THE STANDARDS AND THE MUTCD MAINTAINED.

* WHEN TEMPORARY OPERATING SPEEDS ARE LESS THAN THE POSTED SPEED LIMIT, THE ADVISORY SPEED PLATES (W13-B) SHALL BE USED IN 10 MPH INCREMENTS, UNTIL THE SPEED IS REDUCED TO THE TEMPORARY OPERATING SPEED.

** BUFFER ZONES OF 90 m MINIMUM, 150 m DESIRABLE ARE REQUIRED FOR TANGENT SECTIONS AND SHALL BE INCREASED FOR HORIZONTAL OR VERTICAL CURVES AS DIRECTED BY THE ENGINEER DUE TO SIGHT DISTANCE CONSIDERATIONS

NOTE: DIMENSIONS ARE IN MILLIMETERS UNLESS SPECIFIED OTHERWISE.

6-30-99		DATE		DEPARTMENT OF TRANSPORTATION	
				STATE OF GEORGIA	
				STANDARD TRAFFIC CONTROL DETAIL FOR LANE CLOSURE ON MULTI-LANE DIVIDED HIGHWAY (METRIC)	
				NOV. 1996	
BY	DES.	(SUBMITTED)	<i>James H. Kessel</i> STATE ROAD & AIRPORT DESIGN ENGR.		NUMBER
	TRA.	(APPROVED)			
	CHK.				