

GENERAL NOTES

- THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO AVOID DAMAGE TO BUILDINGS AND ADJACENT PROPERTY. ANY DAMAGE TO ADJACENT BUILDINGS AND/OR PROPERTY SHALL BE REPAIRED AT THE EXPENSE OF THE CONTRACTOR.
- ALL DRIVEWAYS SHALL BE PLACED AS DIRECTED BY THE ENGINEER. CONSTRUCTION OF DRIVEWAYS SHALL BE IN ACCORDANCE WITH GEORGIA STANDARD 6050 OR GEORGIA STANDARD 9031B. ALL DRIVES THAT ARE TO BE RECONSTRUCTED SHALL BE REPLACED IN KIND, I.E. ASPHALT FOR ASPHALT, CONCRETE FOR CONCRETE, OR GRADED AGGREGATE FOR EARTH, AS FOLLOWS:

ASPHALT DRIVES	12.5mm SUPERPAVE, SPREAD RATE 90 kg/m ² 19mm SUPERPAVE, SPREAD RATE 120 kg/m ²
CONCRETE DRIVES	150mm GRADED AGGREGATE OR 25mm SUPERPAVE, SPREAD RATE 130 kg/m ²
EARTH DRIVES	200mm VALLEY CUTTER 100mm GRADED AGGREGATE
- THIS ENTIRE PROJECT IS CONSIDERED TO BE WITHIN AN INSECT INFESTED AREA. THE CONTRACTOR SHALL CONDUCT HIS OPERATIONS IN STRICT COMPLIANCE WITH SECTION 455 OF THE GEORGIA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS.
- ALL REGULATORY, WARNING, DIRECTIONAL, ETC. SIGNS AND STRIPING SHALL BE ERECTED/PLACED AND MAINTAINED IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) TO PROVIDE FOR SAFE AND EFFICIENT TRAFFIC CONTROL DURING CONSTRUCTION OF THE PROJECT. ALL EXISTING SIGNS DESIGNATED (CS) ON THE PLANS FOR RE-USE WILL BE RETAINED IN THEIR CURRENT LOCATION SO LONG AS THEY DO NOT CONFLICT WITH CONSTRUCTION OF THE PROJECT. WHEN CONFLICTS OCCUR THEY WILL BE REMOVED, STORED AND RESET IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND AS DIRECTED BY THE ENGINEER.
- ALL EXISTING DRIVES WHICH ARE TO BE RETAINED AND ALL CROSS STREETS WILL BE PROVIDED INGRESS/EGRESS ROUTES AT ALL TIMES DURING THE CONSTRUCTION ACTIVITIES.
- ALL ITEMS WHICH ARE REMOVED AND NOT TO BE REUSED SHALL BE DISPOSED OF BY THE CONTRACTOR TO ASSURE NO UNSIGHTLY DEBRIS ALONG THE PROJECT DURING CONSTRUCTION. BURIAL ON THE PROJECT WILL NOT BE ALLOWED. THE COST OF DISPOSAL SHALL BE INCLUDED IN THE UNIT PRICE FOR THE VARIOUS REMOVALS.
- AT LOCATIONS WHERE PAVEMENT IS TO BE PLACED ADJACENT TO EXISTING PAVEMENT, WHERE CURBING IS NEARBY TO BE PLACED ADJACENT TO EXISTING PAVEMENT OR ACROSS PAVED PARKING LOTS, OR PAVEMENT IS REQUIRED TO BE REMOVED, IT WILL BE NECESSARY TO SAW CUT A JOINT ON A NEAT LINE ESTABLISHED BY THE ENGINEER TO INSURE A NEAT AND STRAIGHT PAVEMENT EDGE. THE COST OF THIS WORK SHALL BE INCLUDED IN THE UNIT PRICE FOR OTHER ITEMS ON THIS PROJECT. NO SEPARATE PAYMENT WILL BE MADE FOR THIS WORK.
- AT THE INTERSECTION OF THE PROJECT MAINLINE WITH MINOR STREETS AND/OR MAJOR DRIVES WITH EXISTING CURB AND GUTTER, THE PROPOSED 200mm X 150mm CONCRETE CURB AND GUTTER SHALL BE TRANSITIONED TO MATCH THE GRADES AND DIMENSIONS OF THE EXISTING CURB AND GUTTER AT THE LIMITS OF CONSTRUCTION AS SHOWN ON THE PLANS. WHERE THERE IS NO EXISTING CURB AND GUTTER, THE PROPOSED CURB AND GUTTER WILL END AT THE LOCATION AS SHOWN ON THE PLANS WITH THE CURB HEIGHT BEING TRANSITIONED TO MATCH THE GUTTER LINE AT THAT POINT. IN BOTH CASES, THE NEW PAVEMENT SHALL TRANSITION SMOOTHLY INTO THE EXISTING PAVEMENT.
- CURB CUT RAMPS SHALL BE REQUIRED ON THIS PROJECT. THEY WILL CONFORM TO GEORGIA STANDARD 9031B.
- THE TOTAL DISTURBED AREA ASSOCIATED WITH THIS PROJECT WILL BE APPROXIMATELY 18 HECTARES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE SECURITY OF HIS PLANT AND FOR THE ACTIONS OF HIS EMPLOYEES. ALL OPERATIONS ON AND ACCESS TO THE CONFINES OF FORT STEWART WILL BE UNDER THE JURISDICTION OF THE POST PROVOST MARSHALL. ALL ORDERS AND DIRECTIONS GIVEN, AND DECISIONS MADE, BY THE PROVOST MARSHALL SHALL BE FOLLOWED WITHOUT RECOURSE.
- ALL CLEARING DEBRIS AND SPOIL MATERIAL SHALL BE DISPOSED OF IN AN ON-SITE SPOIL DISPOSAL AREA, LOCATED APPROXIMATELY 2 MILES FROM THE LOCATION OF THE WORK. THE CONTRACTOR WILL MAINTAIN HAUL ROUTES AND PITS AND RETURN THEM TO A USABLE CONDITION WHEN THE PROJECT IS COMPLETE.
- A STAGING AREA WITHIN THE PROJECT LIMITS SHALL BE DESIGNATED BY THE ARMY FOR STORAGE OF EQUIPMENT AND MATERIALS. THE CONTRACTOR SHALL CONTACT TIM WHITEHURST OF THE FORT STEWART DEPARTMENT OF PUBLIC WORKS (912-767-7898) FOR LOCATION. ALL EQUIPMENT MUST BE RETURNED TO THE STAGING AREA AT THE END OF CONSTRUCTION ACTIVITIES EACH DAY. NO EQUIPMENT CAN BE LEFT ON THE PROJECT.
- THE ENGINEER'S AND CONTRACTOR'S FIELD OFFICES WILL BE LOCATED AT THE SAME LOCATION AS THE STAGING AREA. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL UTILITY HOOK-UPS TO THE OFFICES.

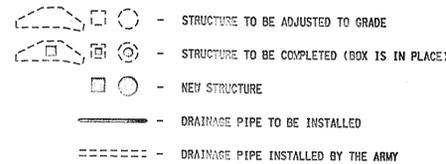
EARTHWORK, GRADING BASE AND SURFACING NOTES

- BULK EXCAVATION AND EMBANKMENT CONSTRUCTION FOR THE PROJECT HAS BEEN COMPLETED BY THE FORCES OF THE UNITED STATES ARMY. EXCEPT FOR SPECIAL AREAS, ALL OF THE ROADBEDS ARE COMPLETE. HOWEVER, THE CONTRACTOR WILL STILL HAVE CERTAIN EARTHWORK AND GRADING RESPONSIBILITIES AS FOLLOWS:
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY CUT OR FILL REQUIRED TO REACH SUBGRADE ELEVATIONS IN ACCORDANCE WITH THE PROFILE AND TYPICAL SECTION.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR EXCAVATION OR FLUSHING OF THE SHOULDERS IN ACCORDANCE WITH THE TYPICAL SECTION OR AS DIRECTED BY THE ENGINEER TO FACILITATE PROPER DRAINAGE.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE CONSTRUCTION OF THE SHOULDERS IN THESE AREAS WHERE THE EXISTING EARTHWORK IS NOT SUFFICIENT TO MEET THE TYPICAL SECTION UNLESS THE FIELD ENGINEER DETERMINES THAT ADJUSTMENT SHALL BE MADE TO MATCH EXISTING.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONSTRUCTING OR RECONSTRUCTING DITCHES WHERE NEEDED TO MAINTAIN THE DRAINAGE SYSTEM.

THE EARTHWORK ASSOCIATED WITH THE ABOVE SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR GRADING, COMPLETE, ITEM NUMBER 210-0100.
- EXCAVATED UNSUITABLE MATERIAL MAY BE USED TO FLUSH AND FLATTEN SLOPES AT THE DIRECTION OF THE ENGINEER.
- BORROW MATERIAL REQUIRED FOR THE COMPLETION OF THIS PROJECT WILL BE OBTAINED ON-SITE AT A PIT DESIGNATED BY THE ARMY. THE PIT LOCATION IS APPROXIMATELY 2 MILES FROM THE SITE OF THE WORK. THE MATERIAL WILL BE PROVIDED AT NO COST TO THE CONTRACTOR. ALL EXCAVATION AND TRANSPORTATION COSTS ASSOCIATED WITH THE BORROW MATERIAL SHALL BE INCLUDED IN THE PRICE BID FOR GRADING, COMPLETE. THE CONTRACTOR IS TO MAINTAIN THE PIT AREA AND HAUL ROUTES AND SHALL RESTORE THEM TO A USABLE CONDITION WHEN THE PROJECT IS COMPLETE. THE CONTRACTOR SHALL CONTACT TIM WHITEHURST OF THE FORT STEWART DEPARTMENT OF PUBLIC WORKS (912-767-7898) FOR EXACT LOCATION AND GUIDANCE.
- THE SUPERPAVE MIX DESIGN LEVEL FOR THE PROJECT IS "C".
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR COMPACTING THE EXISTING OR NEW EARTHWORK TO THE REQUIREMENTS OF THE SPECIFICATIONS. THE COST FOR THIS WILL BE INCLUDED IN THE UNIT PRICE BID FOR GRADING COMPLETE.

DRAINAGE NOTES

- TOPS OF EXISTING STORM MANHOLES, CURB INLETS, AND DITCH INLETS SHALL BE ADJUSTED DURING CONSTRUCTION TO BETTER MEET FIELD CONDITIONS.
- THE CONTRACTOR SHALL CLEAN ALL NEW PIPES AND EXISTING PIPES OF SILT AND DEBRIS WITHIN PROJECT LIMITS. COST TO BE INCLUDED IN LUMP SUM PRICE BID FOR "EROSION CONTROL". THIS CLEANING WILL BE REPEATED AS REQUIRED TO ENSURE THAT THE DRAINAGE SYSTEM IS COMPLETELY FUNCTIONAL AT TIME OF FINAL ACCEPTANCE. NO EXTRA COMPENSATION WILL BE PROVIDED FOR CLEANING RESULTING FROM THE CONTRACTOR'S FAILURE TO PROTECT THE PROJECT FROM EROSION OR SEDIMENTATION.
- ALL EXISTING STORM DRAINAGE FACILITIES ARE UNDER THE JURISDICTION OF THE FT. STEWART DEPARTMENT OF PUBLIC WORKS. THE CONTRACTOR SHALL COORDINATE WITH THE DEPARTMENT OF PUBLIC WORKS STORM DRAINAGE DEPARTMENT AT 650-7655.
- ALL EXISTING STORM DRAINAGE FACILITIES ARE UNDER THE JURISDICTION OF THE FT. STEWART DEPARTMENT OF PUBLIC WORKS. THE CONTRACTOR SHALL COORDINATE WITH THE DEPARTMENT OF PUBLIC WORKS STORM DRAINAGE DEPARTMENT AT 650-7655.
- THE CONTRACTOR SHALL COMPLETE OR ADJUST STORM DRAINAGE STRUCTURES ALREADY IN PLACE, EITHER COMPLETELY OR PARTIALLY. THIS WILL INVOLVE RAISING TOPS TO MEET FINISHED GRADE OR COMPLETING CATCH BASIN CONSTRUCTION WHERE THE PRECAST CONCRETE BOX AND STORM PIPES ARE IN PLACE. THE REQUIRED TOP FOR THE CATCH BASINS ARE ON SITE AND WILL NEED TO BE INSTALLED; THROTS WILL NEED TO BE POURED AND PEDISTALS PLACED. DRAINAGE STRUCTURE WORK IS INDICATED ON THE PROJECT PLANS PER THE FOLLOWING LEGEND:



STRUCTURES TO BE ADJUSTED TO GRADE WILL BE PAID FOR UNDER ITEM 611-3055, "ADJUST MINOR STRUCTURE TO GRADE". NEW STRUCTURES TO BE INSTALLED SHALL BE PAID FOR UNDER THE APPROPRIATE LISTED ITEM. CATCH BASINS TO BE COMPLETED SHALL BE PAID FOR UNDER ITEM 611-3000, "RECONSTRUCT CATCH BASIN, GROUP 1".

UTILITY NOTES

A. GENERAL

- THE CONTRACTOR'S ATTENTION IS CALLED TO ARTICLE 107-21, "COOPERATION WITH UTILITIES", AS COORDINATION WITH UTILITIES WILL BE REQUIRED ON THIS PROJECT.
- ALL KNOWN UTILITY FACILITIES ARE SHOWN SCHEMATICALLY ON THE HIGHWAY PLANS AND ARE NOT NECESSARILY ACCURATE AS TO PLAN OR ELEVATION. UTILITY FACILITIES SUCH AS SERVICE LINES OR UNKNOWN FACILITIES NOT SHOWN ON THE PLANS WILL NOT RELIEVE THE CONTRACTOR OF HIS RESPONSIBILITY UNDER THIS REQUIREMENT EXCEPT AS NOTED BELOW. "EXISTING UTILITY FACILITIES" MEANS ANY UTILITY THAT EXISTS ON THE HIGHWAY PROJECT IN ITS ORIGINAL, RELOCATED OR NEWLY INSTALLED POSITION. THE CONTRACTOR WILL NOT BE RESPONSIBLE FOR THE COST OF REPAIRS TO DAMAGED UTILITY FACILITIES, OTHER THAN SERVICE LINES FROM STREET MAINS TO ADJUTING PROPERTY WHEN SUCH FACILITIES ARE NOT SHOWN ON THE HIGHWAY PLANS AND WHOSE EXISTENCE IS UNKNOWN TO THE CONTRACTOR PRIOR TO THE DAMAGES OCCURRING, PROVIDING THAT THE ENGINEER DETERMINES THE CONTRACTOR HAS OTHERWISE FULLY COMPLIED WITH THE PLANS AND SPECIFICATIONS.
- THE ROADWAY CONTRACTOR SHALL NOT BE REQUIRED TO RELOCATE ANY UTILITIES. ALL UTILITIES WHICH ARE IN CONFLICT WITH THE CONSTRUCTION ACTIVITIES ARE TO BE RELOCATED BY THE RESPONSIBLE UTILITY OWNER. THE CONTRACTOR WILL BE RESPONSIBLE FOR COORDINATING THESE ACTIVITIES WITH HIS WORK.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR VERIFYING THE LOCATION AND ELEVATION OF ALL UTILITY FACILITIES ON THE PROJECT. THE CONTRACTOR WILL BE RESPONSIBLE FOR ANY DAMAGES TO THESE FACILITIES, EXCEPT AS COVERED BY NOTE 2 ABOVE.
- ALL EXISTING TELEPHONE, ELECTRIC OR GAS MANHOLE AND / OR VALVE TOPS IN THE CONSTRUCTION AREA SHALL BE ADJUSTED BY THE RESPONSIBLE UTILITY COMPANY TO MATCH NEW GRADES. ALL WATER VALVES AND SANITARY SEWER MANHOLE TOPS IN THE PROJECT AREA SHALL BE ADJUSTED BY THE CONTRACTOR UNDER THE ROADWAY CONTRACT. PAYMENT FOR THIS WORK WILL BE UNDER ITEM 611-8140, "ADJUST WATER VALVE BOX TO GRADE" AND 611-8090, "ADJUST MANHOLE TO GRADE".
- THE CONTRACTOR SHALL CALL THE STATEWIDE "CALL BEFORE YOU DIG" NUMBER (1-800-282-7411) PRIOR TO BEGINNING ANY EXCAVATION ON THE PROJECT. THE CONTRACTOR SHALL ALSO CONTACT TIM WHITEHURST AT THE FORT STEWART DEPARTMENT OF PUBLIC WORKS (912-767-7898) PRIOR TO BEGINNING ANY EXCAVATION ON THE PROJECT.
- THE FOLLOWING UTILITY OWNERS HAVE FACILITIES WHICH MAY CONFLICT WITH CONSTRUCTION ACTIVITIES:

NAME OF CONTACT	TELEPHONE	TYPE OF FACILITY
GENE SMITH	767-2138	WATER/SEWER/GAS (DPW)
RAY COLLIER	767-5373	ELECTRICAL UTILITIES (DPW)
MICHAEL DASHER	767-5466	EXCAVATION PERMITS (DPW)
JERRY DELOACH	368-0216	COASTAL UTILITIES (TELEPHONE)
DEAN OTT	368-5800	BRESNAN COMMUNICATIONS (CABLE)
JOHNNY GRINER	767-4360	DOIM (FIBER OPTICS & MAIN LINE)

ALL WATER, SEWER, GAS AND ELECTRICAL UTILITIES ARE THE RESPONSIBILITY OF THE FORT STEWART DEPARTMENT OF PUBLIC WORKS. CONTACT TIM WHITEHURST OR KEN DAVIS AT 912-767-7898.

STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
GA.	PLH 149-1 (34)	04	227

TRAFFIC CONTROL GENERAL NOTES

- THE CONTRACTOR SHALL COORDINATE REMOVAL AND RELOCATION OF TRAFFIC SIGNALS REQUIRED BY STAGE CONSTRUCTION WITH THE FT. STEWART DEPARTMENT OF PUBLIC WORKS AND THE GEORGIA DEPARTMENT OF TRANSPORTATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING THE LOCATION OF SIGNAL HEADS AS REQUIRED BY STAGE CONSTRUCTION. ALL SALVAGED TRAFFIC SIGNAL HEADS WILL BE DELIVERED TO THE FORT STEWART DEPARTMENT OF PUBLIC WORKS AT THEIR ON-SITE SHOP/STORAGE YARD OR AT SUCH OTHER ON-SITE LOCATION AS MAY BE DIRECTED.
- ALL ROAD CLOSINGS SHALL BE CLEARED A MINIMUM OF FIVE DAYS IN ADVANCE WITH THE FORT STEWART PROVOST MARSHALL. ALL DETOURS, ROAD CLOSINGS AND OTHER TRAFFIC CONTROL MEASURES MUST BE APPROVED BY BOTH THE GEORGIA DEPARTMENT OF TRANSPORTATION AND THE FORT STEWART PROVOST MARSHALL PRIOR TO IMPLEMENTATION.