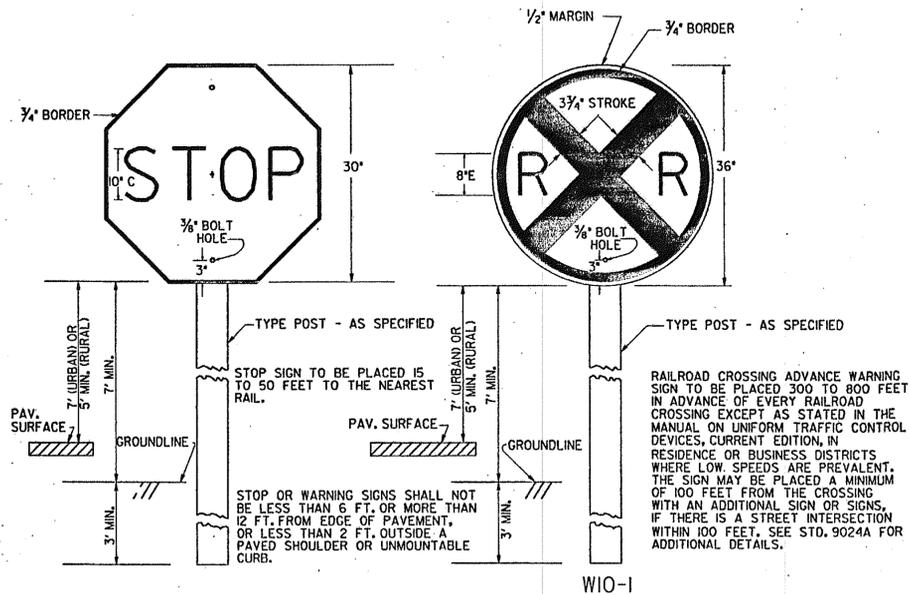


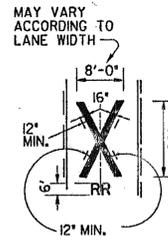
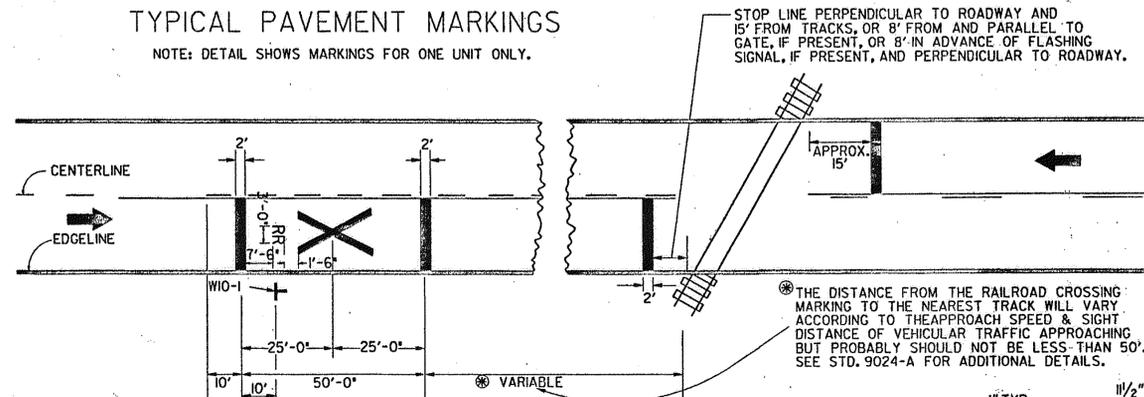
DETAILS OF STOP AND RAILROAD ADVANCE WARNING SIGN



NOTES:

- RAILROAD ADVANCE WARNING SIGNS SHALL HAVE A YELLOW REFLECTORIZED BACKGROUND WITH BLACK LEGEND, SYMBOL, AND BORDER, BACKGROUND SHALL BE TYPE V REFLECTIVE SHEETING, SIGN PLATE SHALL BE ALUMINUM, 0.08 INCH MINIMUM THICKNESS.
- STOP SIGNS SHALL BE OCTAGONAL IN SHAPE, SHALL HAVE RED TRANSPARENT DURABLE INK PRINTED ON TYPE 'V' REFLECTIVE SHEETING, THE BORDER SHALL BE REFLECTORIZED WHITE, BACKGROUND SHALL BE REFLECTORIZED WITH REFLECTIVE SHEETING, SIGN PLATE SHALL BE ALUMINUM, 0.08 INCH ALUMINUM THICKNESS.
- STOP SIGNS AND RAILROAD ADVANCE WARNING SIGNS SHALL CONFORM TO THE SPECIFICATIONS AND REQUIREMENTS SET FORTH IN THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITION, AND THE GEORGIA STANDARD SPECIFICATIONS AND/OR THE SPECIAL PROVISIONS.

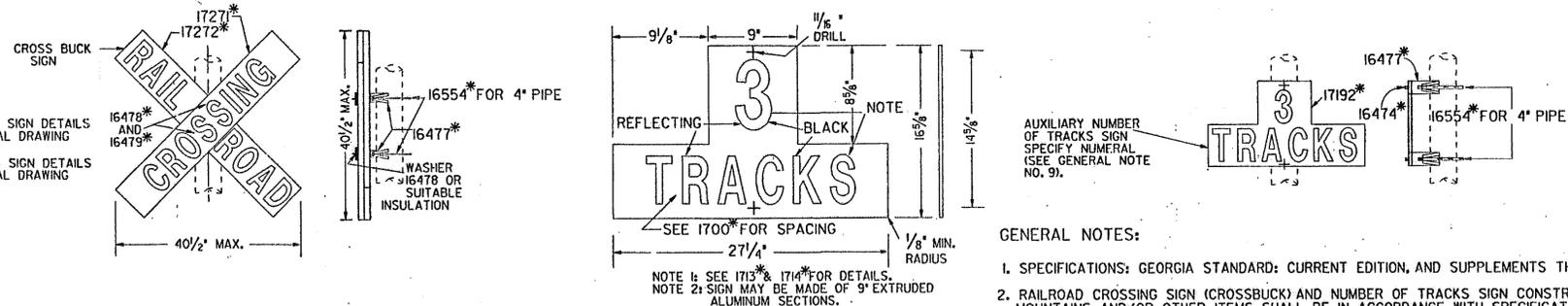
TYPICAL PAVEMENT MARKINGS



NOTES:

- A 3-LANE ROADWAY SHOULD BE MARKED WITH A CENTERLINE FOR 2-LANE APPROACH OPERATION ON THE APPROACH TO A CROSSING.
- ON MULTI-LANE ROADS THE TRANSVERSE BANDS SHALL EXTEND ACROSS ALL APPROACH LANES, AND INDIVIDUAL R X R SYMBOLS SHALL BE USED IN EACH APPROACH LANE.
- ALL MARKINGS AND ALL CONSTRUCTION TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE 'MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES', CURRENT EDITION, & ANY SUPPLEMENTS THERETO.
- PAYMENT FOR NEW PAVEMENT MARKINGS INCLUDES OBLITERATING EXISTING PAVEMENT MARKINGS, WHERE NECESSARY.
- CENTERLINE, LANELINES & EDGELINES APPROACH MARKINGS WILL BE ADDED, WHERE NECESSARY, SKIP WHITE TRAFFIC STRIPES SHALL BE USED TO MARK LANE LINES ON MULTI-LANE ROADS (NOT SHOWN).
- ALL OF THE ABOVE DESCRIBED AND SHOWN PAVEMENT MARKINGS SHALL BE PLACED AT THE DIRECTION OF THE DISTRICT TRAFFIC ENGINEER.

DETAILS OF RAILROAD CROSSING SIGNS USING REFLEX REFLECTING SHEET EXTRUDED ALUMINUM

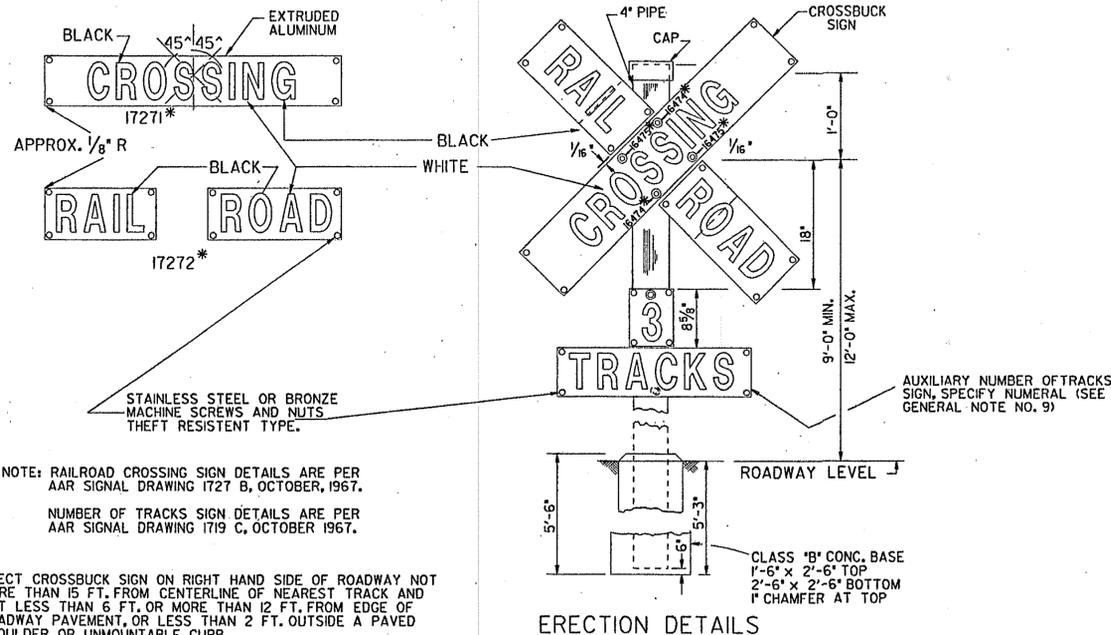


NOTES:

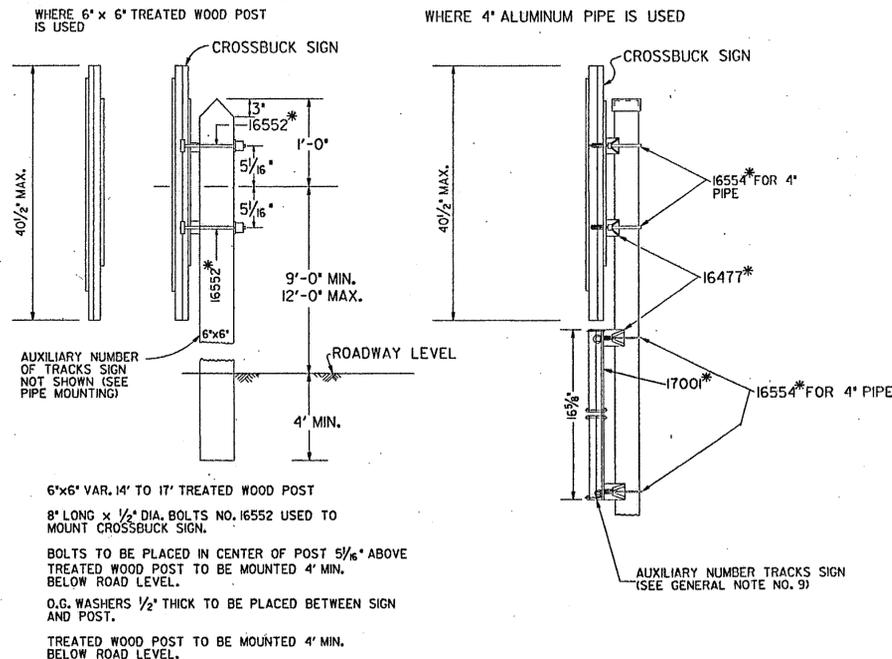
- RAILROAD CROSSING SIGN DETAILS ARE PER AAR SIGNAL DRAWING 1727B, 1727C, AND 16479*. NUMBER OF TRACKS SIGN DETAILS ARE PER AAR SIGNAL DRAWING 1719 C.

*AAR SIGNAL DRAWING NUMBER

DETAILS OF RAILROAD CROSSING SIGNS



TYPICAL MOUNTINGS



GENERAL NOTES:

- SPECIFICATIONS: GEORGIA STANDARD: CURRENT EDITION, AND SUPPLEMENTS THERETO.
- RAILROAD CROSSING SIGN (CROSSBUCK) AND NUMBER OF TRACKS SIGN CONSTRUCTION DETAILS, MOUNTAINS, AND/OR OTHER ITEMS SHALL BE IN ACCORDANCE WITH SPECIFICATION OF THE ASSOCIATION OF AMERICAN RAILROADS.
- WHERE STANDARD 9023A CROSSBUCK SIGN IS SPECIFIED, STANDARD 9023A WARNING SIGN SHALL BE PLACED ALSO.
- A 90° CROSSBUCK SIGN SHALL BE USED ONLY.
- LOCATE CROSSBUCK SIGN AT SUCH POINTS AS WILL ADMIT THE BEST VIEW BY PERSONS APPROACHING THE CROSSINGS.
- A DISTANCE OF 100 FT. SHALL BE ASSUMED TO SEPARATE TRACKS BEFORE AN ADDITIONAL CROSSBUCK SIGN IS CONSIDERED, UNLESS LOCAL CONDITIONS REQUIRE OTHERWISE.
- CROSSBUCK SIGN AND STOP SIGN SHALL BE PLACED ON SEPARATE POSTS.
- CROSSBUCK SIGN AND NUMBER OF TRACKS SIGN SHALL HAVE A WHITE BACKGROUND WITH BLACK LETTERS AND SHALL BE REFLECTORIZED BY REFLECTIVE COATING ON THE BACKGROUND. REFLECTORIZED CROSSBUCK SIGNS CONSIST OF R.R. CROSSING SIGN COMPLETE NO. 1726, 17262 REFLECTORIZED NUMBER OF TRACKS SIGN CONSISTS OF TRACKS SIGN COMPLETE NO. 1719.
- IF THERE ARE TWO OR MORE TRACKS, AN AUXILIARY SIGN INDICATING THE TOTAL NUMBER OF TRACKS CROSSED, INCLUDING SIDINGS, SHALL BE MOUNTED BELOW THE CROSSBUCK SIGN.
- WOOD SHALL BE TREATED IN ACCORDANCE WITH THE GEORGIA STANDARD SPECIFICATIONS FOR PRESERVATIVE TREATMENT OF TIMBER PRODUCTS.
- THE TYPICAL PAVEMENT MARKINGS MAY BE VARIED IF IN ACCORDANCE WITH 'STANDARD ALPHABETS FOR HIGHWAY SIGNS AND PAVEMENT MARKINGS' AND APPROVED BY THE ENGINEER.

DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA			
STANDARD RAILROAD GRADE CROSSING SIGNS & MARKINGS			
NO SCALE	REV. & REDR.	NOV., 1999	
REV. & REDR. (SUBMITTED)	TR. (APPROVED)		NUMBER 9023A
CHK.	CHIEF ENGINEER		