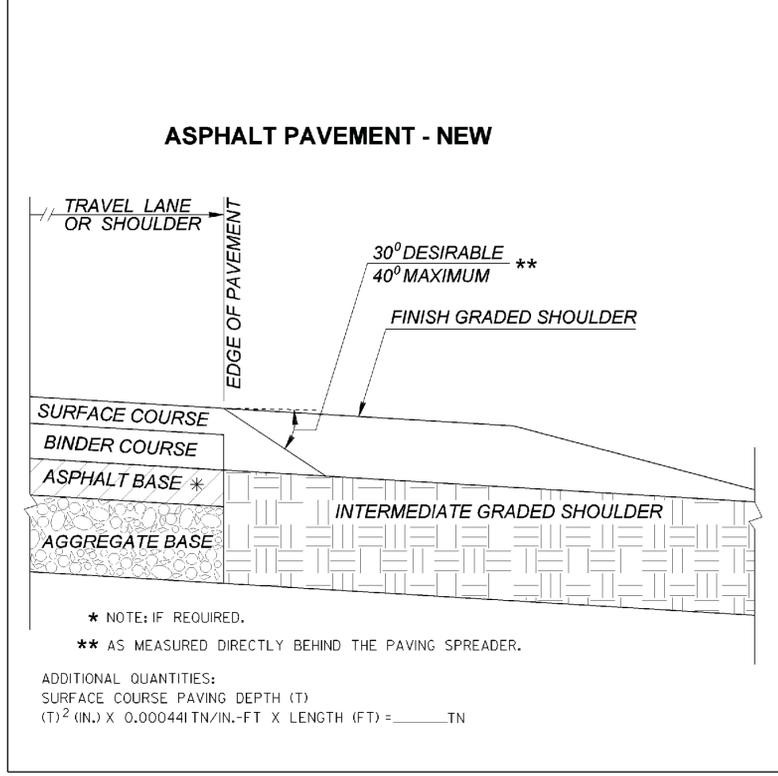
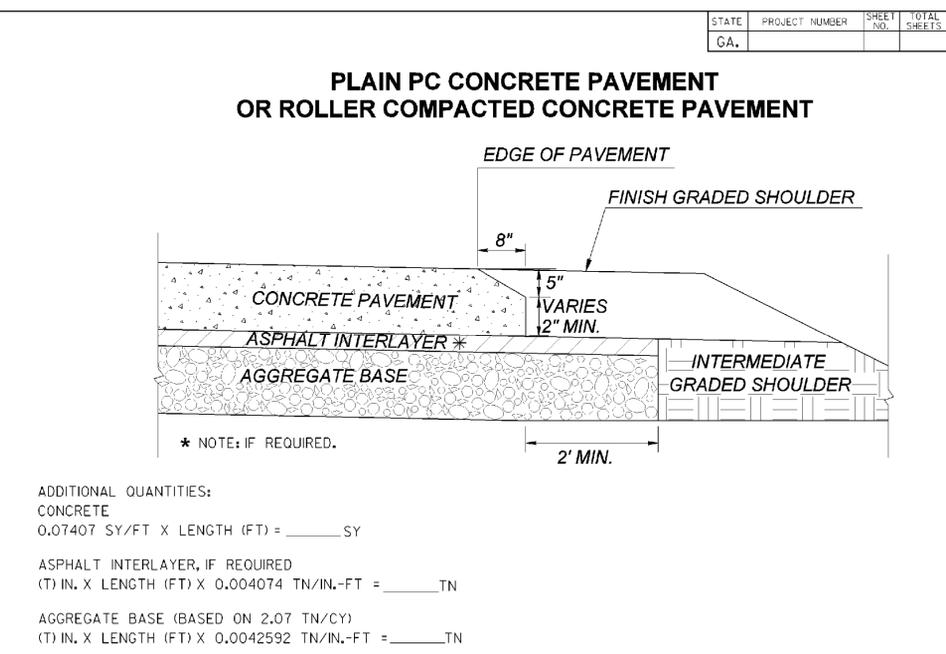
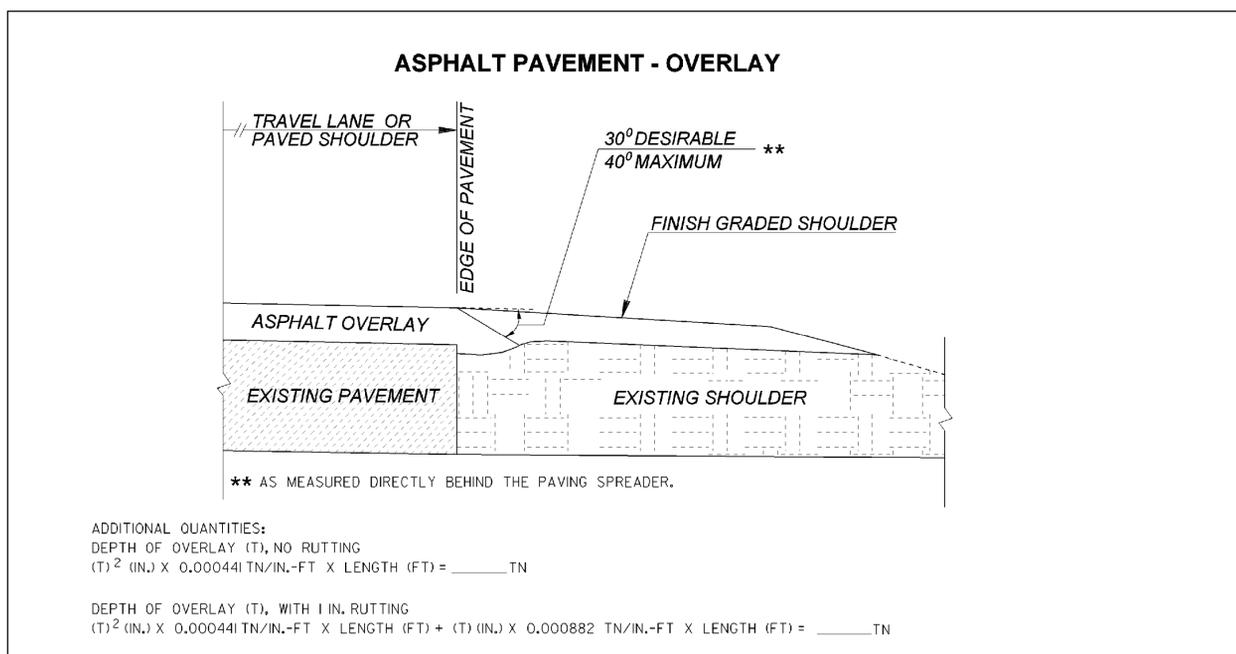


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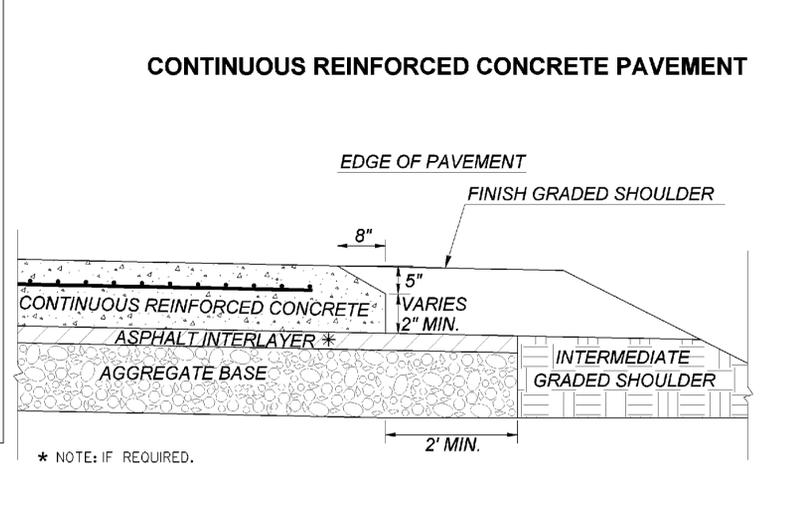
GENERAL NOTES:
 1. THE SAFETY EDGE SHALL BE CONSTRUCTED AS AN INTEGRAL OPERATION OF THE ROADWAY PAVEMENT PLACEMENT PROCESS.
 (ASPHALT PAVEMENT)

2. USE AN APPROVED MECHANICAL DEVICE THAT WILL:
 1. APPLY COMPACTIVE EFFORT TO THE ASPHALT MIXTURE TO ELIMINATE OBJECTABLE VOIDS AS THE MIXTURE PASSES THROUGH THE WEDGE DEVICE.
 2. PRODUCE A WEDGE WITH A UNIFORM TEXTURE, SHAPE, AND DENSITY WHILE AUTOMATICALLY ADJUSTING TO VARYING HEIGHTS ENCOUNTERED ALONG THE ROADWAY SHOULDER.

3. A SINGLE-PLATE STRIKE-OFF METHOD SHALL NOT BE USED FOR BITUMINOUS PAVING, AS THE SINGLE-PLATE STRIKE-OFF METHOD HAS BEEN FOUND TO PRODUCE A NON-DURABLE EDGE.

4. COMPACTION OF THE EDGE SHOULD NOT BE DONE WITH THE FIRST PASS OF THE ROLLER; WITH THE ROLLER STAYING OFF THE EDGE AT LEAST 6 INCHES. THIS IS IN ORDER TO ALLOW THE EDGE MIX TO SLIGHTLY COOL PRIOR TO COMPACTION.

5. SHORT SECTIONS OF HANDWORK ARE ALLOWED, WHEN NECESSARY, FOR TRANSITIONS AND TURNOUTS.



#-5-1		DATE		DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA	
REVISED ANGLE TOLERANCE ANGLE & ADDED NOTE.		REVISION		CONSTRUCTION DETAIL PAVEMENT EDGE TREATMENT ASPHALT AND CONCRETE PAVEMENT	
NO SCALE		SEPTEMBER 2011		NUMBER P-7	
TC	BY	DES. DRW. CHK. REVIEW	B.A.S. C.L.O. D.E.P. B.A.S.		

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REVISION DATES	

STATE OF GEORGIA
 DEPARTMENT OF TRANSPORTATION
 OFFICE: INNOVATIVE PROGRAM DELIVERY
CONSTRUCTION DETAILS