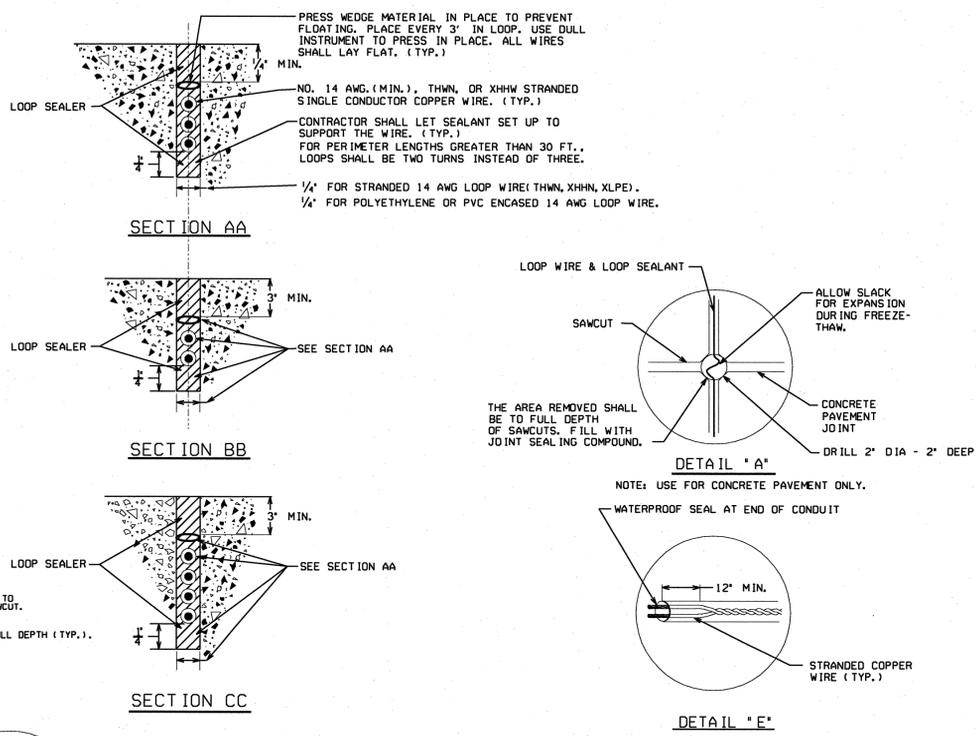
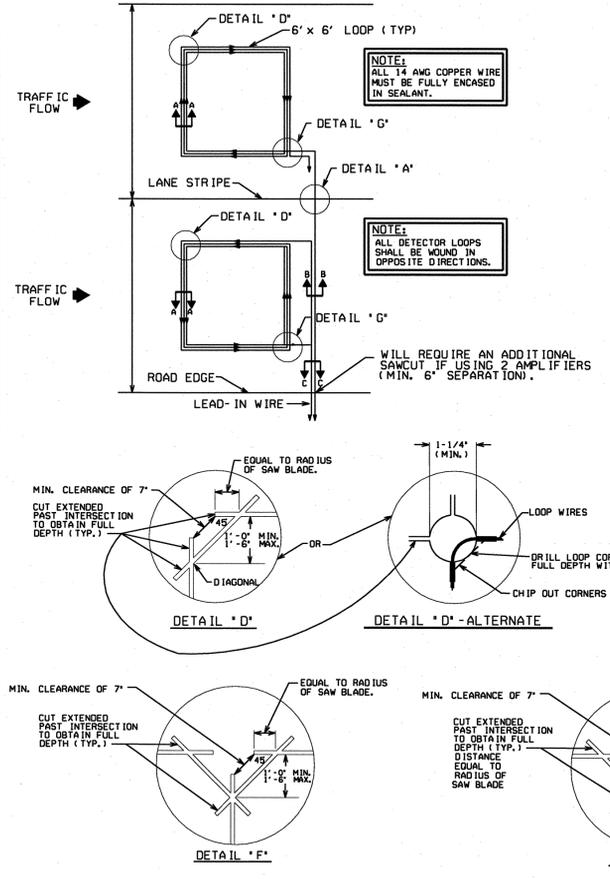
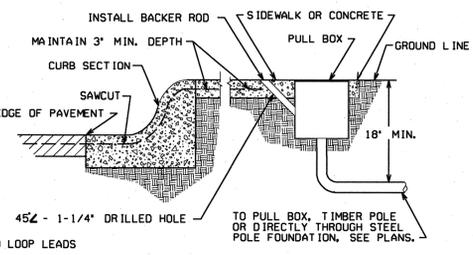


## INDUCTIVE LOOP VEHICLE DETECTOR DETAILS USING STRANDED COPPER WIRE

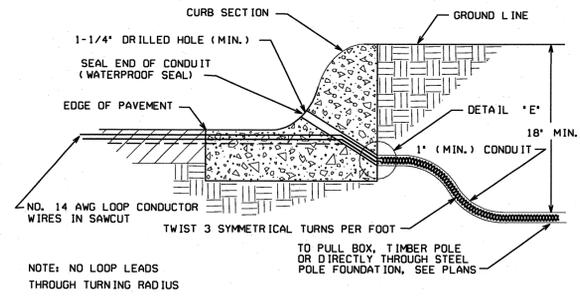
### PLAN VIEW OF STANDARD LOOP SAW CUTS



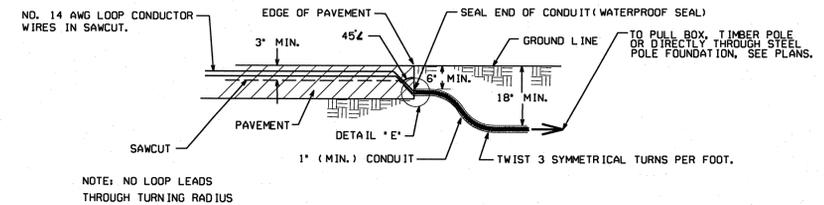
### TYPICAL CURB DETAIL (WITH SIDEWALK)



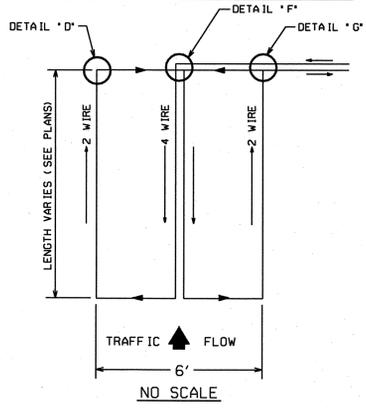
### TYPICAL CURB DETAIL (WITHOUT SIDEWALK)



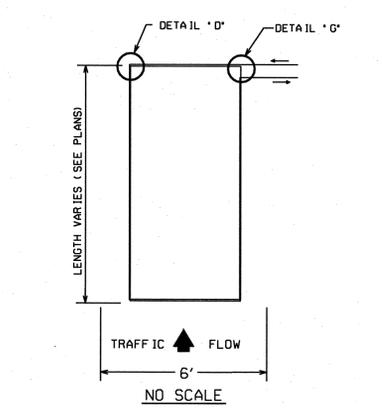
### DETAIL WHERE NO CURB EXISTS



### PLAN VIEW OF QUADRUPOLE



### PLAN VIEW OF STANDARD LOOP



### LOOP WIRE CONFIGURATION

THE DOUBLE LAYER CONFIGURATION (2-4-2) SHOWN IS A MINIMUM DESIGN FOR NORMAL INSTALLATIONS WHEN REQUIRED BY THE PLANS.

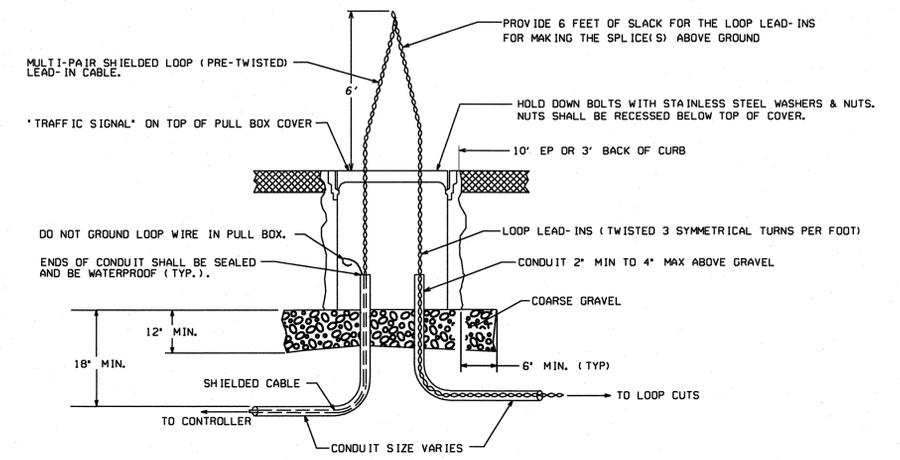
### LOOP WIRE CONFIGURATION

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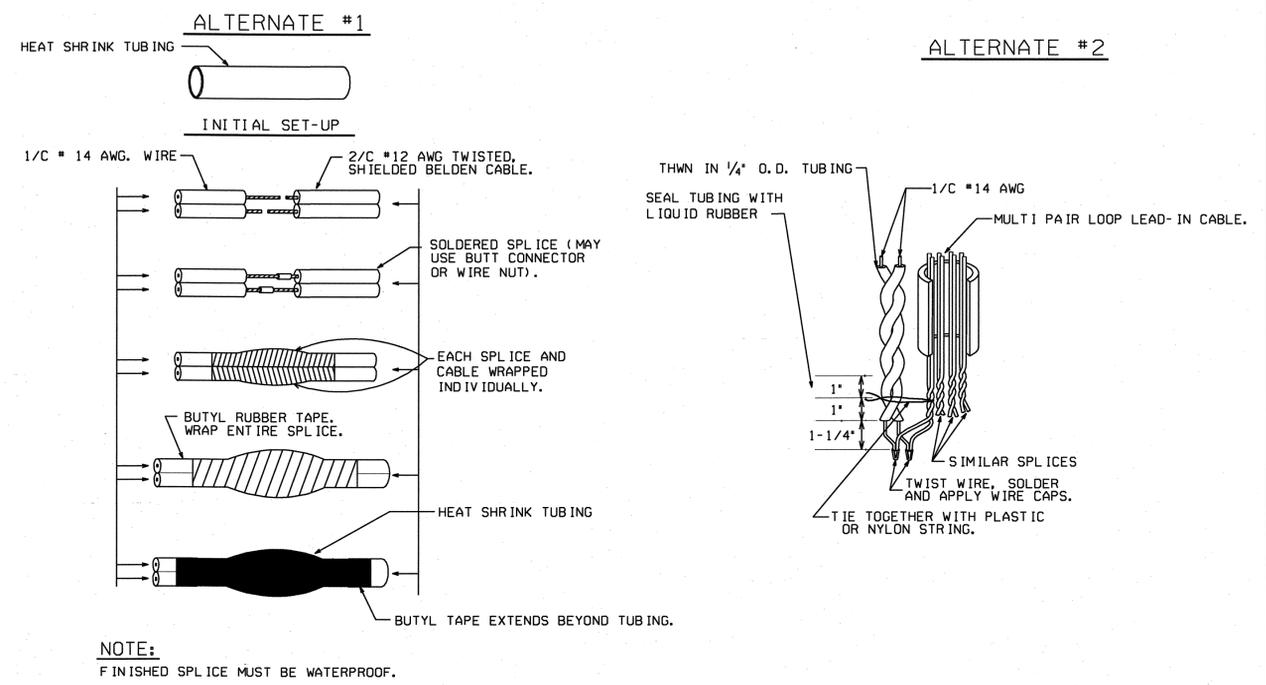
*Guidelines For Usage On Metric Projects*

When these details are incorporated into plans and/or projects that are being prepared or constructed in metric units, exact or precise conversion to metric units is not required. The dimensions shown that are in feet and inches may be converted to corresponding metric units using the following "Rounded-Off" conversion factors: 1" = 25mm, 4" = 100mm, and 1/2" or 1" = 300mm. All measurement notes that refer to linear feet and square yards shall be interpreted to mean linear meters and square meters.

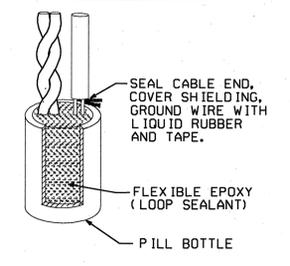
### PULL BOX-SPLICE DETAILS



### SPLICE DETAILS



### FINISHED SPLICE



DATE		REVISION DESCRIPTION		DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA	
REV. BY:				DETAIL NUMBER TS-01	
				APRIL 2010	
NOT TO SCALE - REPORT ERRORS					