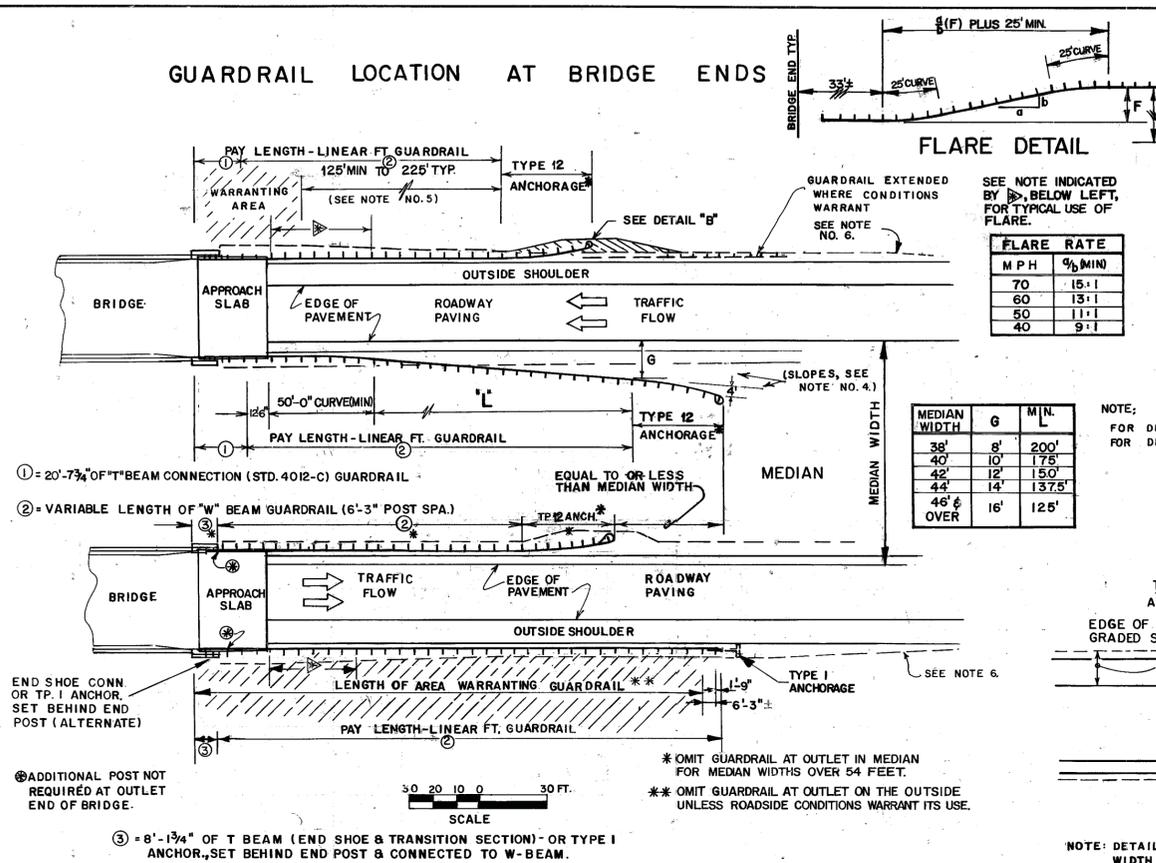
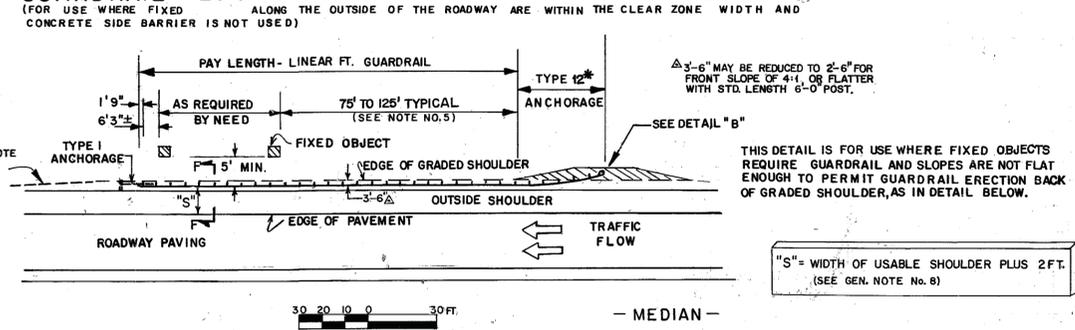


RELEASED FOR CONSTRUCTION - 12/05/11

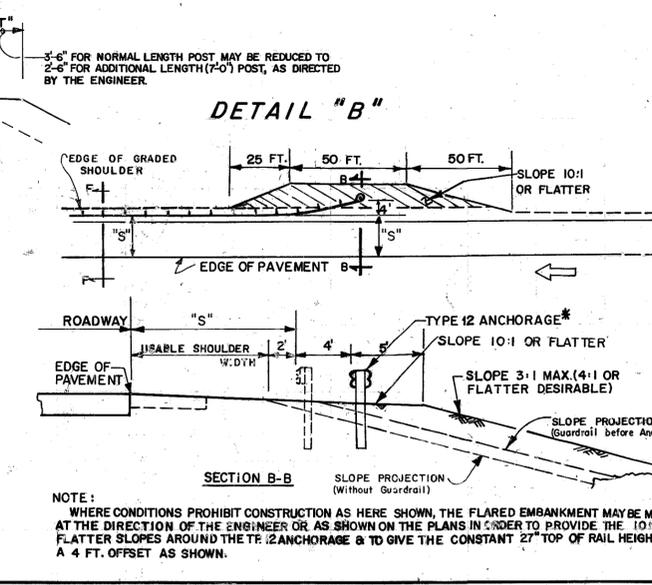
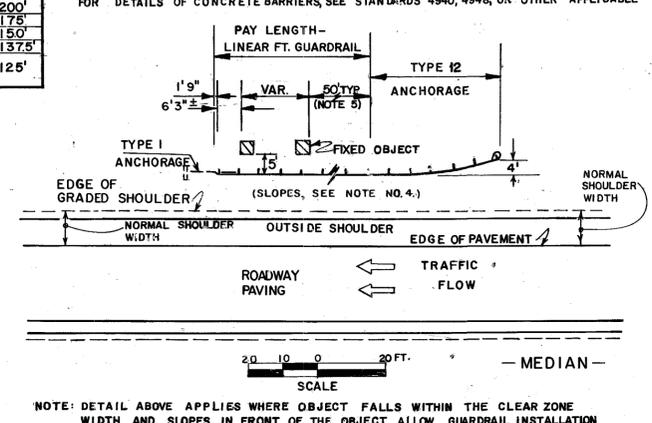
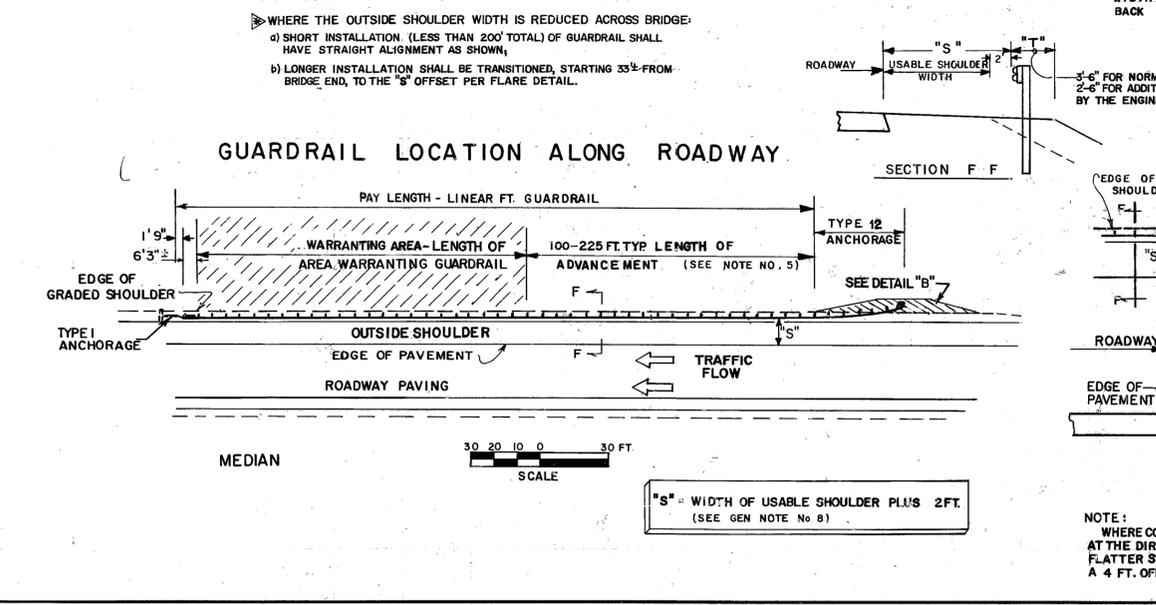
GUARDRAIL LOCATION AT BRIDGE ENDS



GUARDRAIL LOCATION IN FRONT OF FIXED OBJECTS



GUARDRAIL LOCATION ALONG ROADWAY



- GENERAL NOTES:**
- SPECIFICATIONS: GEORGIA STANDARD, CURRENT EDITION & SUPPLEMENTS THERETO.
 - POST SPACINGS SHALL BE 6'-3" C. TO C. EXCEPT WHERE REDUCED SPACINGS ARE SPECIFIED AT BRIDGE ENDS OR AT FIXED HAZARDS.
 - FOR DETAILS OF GUARDRAIL, ACCESSORIES, POSTS, OFFSET BLOCKS, ANCHORAGES, ETC., SEE APPLICABLE STANDARD AND/OR CONSTRUCTION DETAILS.
 - NEGATIVE SLOPES IN FRONT OF GUARDRAIL AND TYPE 12 ANCHORAGES SHALL BE 10:1 OR FLATTER, ON ALL NEW INSTALLATIONS.
 - GUARDRAIL SHALL BEGIN IN ADVANCE OF AN OBJECT OR WARRANTING AREA SUFFICIENT LENGTH TO PREVENT VEHICLE PENETRATION BEHIND THE RAIL INTO PROTECTED AREA. THE TYPICAL LENGTHS OF ADVANCEMENT SHOWN MAY BE INCREASED OR DECREASED WHEN SHOWN IN THE PLANS, OR WHERE DIRECTED BY THE ENGINEER BECAUSE OF SPEED DESIGN, ROADSIDE GEOMETRICS, GRADES, SIZE OF HAZARD, OR OTHER CONDITIONS. IF FURTHER INFORMATION IS DESIRED, SEE GA. D.O.T. "GUIDELINES FOR DETERMINING GUARDRAIL NEED, LOCATION AND STANDARDS" OR AASHTO "ROADSIDE DESIGN GUIDE."
 - WHERE GUARDRAIL IS REQUIRED ON THE SHOULDER, THE SHOULDER WILL BE GRADED WIDER AS SHOWN IN DETAIL "B".
 - GAPS OF LESS THAN 200 FT. BETWEEN GUARDRAIL INSTALLATIONS SHOULD BE AVOIDED EXCEPT WHERE JUSTIFIED BY LOCAL CONDITIONS.
 - "S" IS THE OFFSET TO FACE OF GUARDRAIL THIS WILL BE TWO FT. GREATER THAN THE NORMAL USABLE SHOULDER WIDTH (WITHOUT GUARDRAIL), HOWEVER, IF THE NORMAL GRADED SHOULDER WIDTH (WITHOUT GUARDRAIL) IS GREATER THAN REQUIRED BY AASHTO, "S" DISTANCE MAY BE ESTABLISHED AS TWO (2) FT. GREATER THAN THE AASHTO SHOULDER WIDTH.
 - OFFSET FROM RAIL FACE TO HAZARD FACE SHALL BE 4'-3" DESIRABLE AND 4'-3" MINIMUM THIS OFFSET MAY BE REDUCED TO 3'-0" MIN. BY USING 3'-1 1/2" POST SPACINGS IN FRONT OF HAZARD WITH A MINIMUM OF 7 SUCH SPACINGS IN ADVANCE OF HAZARD.
- SPECIAL NOTE:** LOCATION AND QUANTITIES GIVEN IN THE PLANS FOR GUARDRAIL AND ANCHORAGES ARE ESTIMATES MADE FROM OFFICE COMPUTATIONS. A FINAL DETERMINATION AS TO LOCATIONS AND QUANTITIES OF GUARDRAIL AND ANCHORAGES WILL BE MADE BY THE ENGINEER OR A REPRESENTATIVE FROM THE OFFICE OF TRAFFIC OPERATIONS AFTER CONSTRUCTION OF ROADWAY.

FOR APPROVED ALTERNATE, MEETING NCHRP 350 REQUIREMENTS

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA

STANDARD
GUARDRAIL LOCATION DETAILS
FOR MULTI-LANE DIVIDED HIGHWAYS
(WITH SHOULDERS ADJACENT TO ROADWAY)

SCALE: AS SHOWN REV. & REDR. DEC. 1984

DES. R.M.U.	(SUBMITTED)	NUMBER 4051
DRW. R.M.U.	STATE ROAD & AIRPORT DESIGN ENGR.	
CHK. G.M.E.	(APPROVED)	
PHK. R.K.C.	STATE HIGHWAY ENGINEER	

THE LPA GROUP

TRANSPORTATION CONSULTANTS

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REVISION DATES

STATE OF GEORGIA
DEPARTMENT OF TRANSPORTATION
OFFICE: INNOVATIVE PROGRAM DELIVERY
GEORGIA STANDARDS