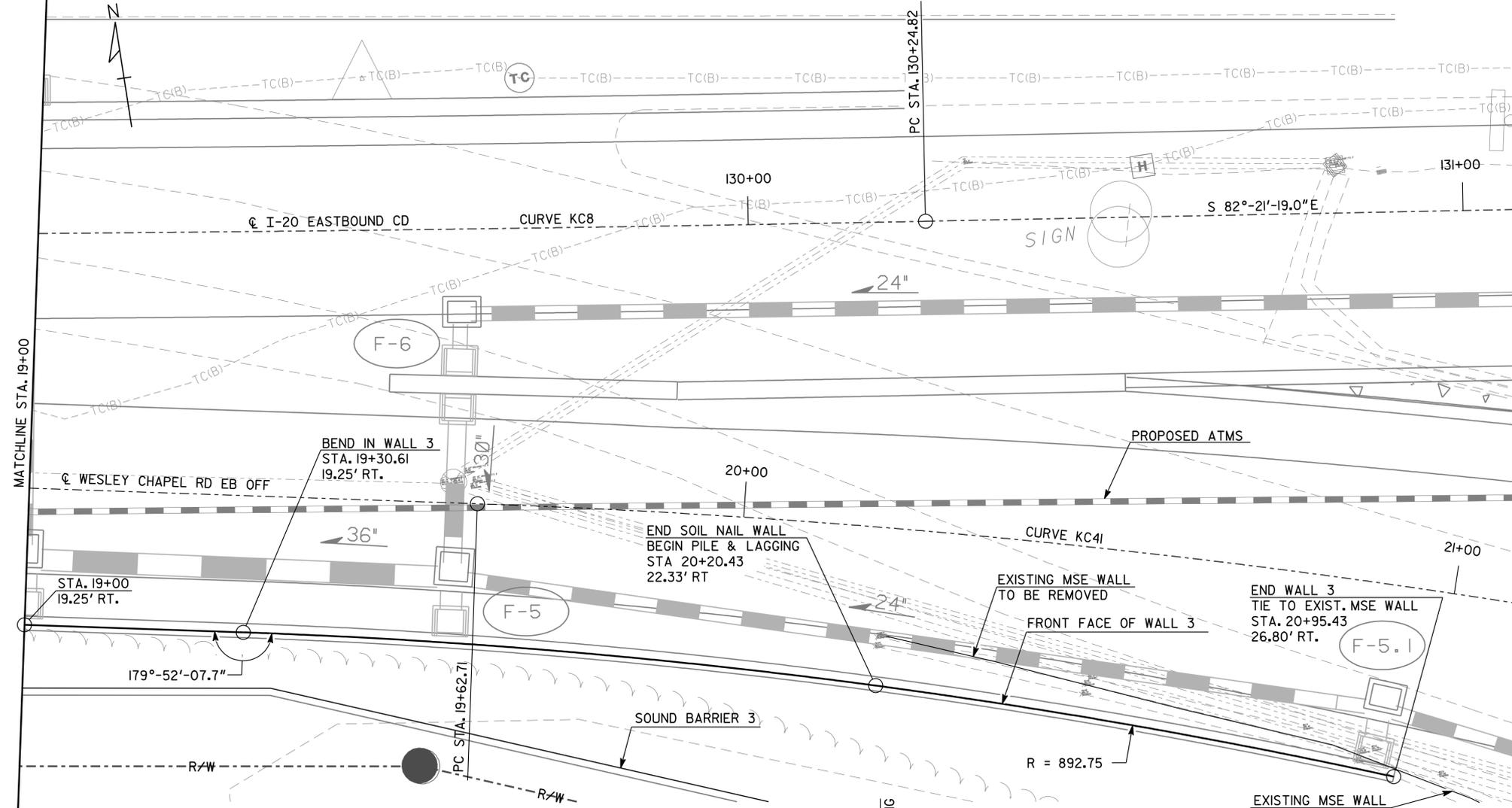
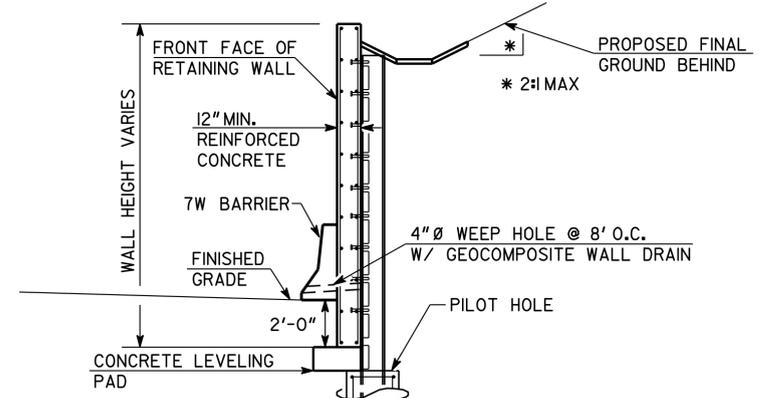


PILE AND LAGGING WALL NOTES

SPECIFICATIONS - GEORGIA STANDARD SPECIFICATIONS, 2001 EDITION AND 2008 SUPPLEMENTAL SPECIFICATIONS, AS MODIFIED BY CONTRACT DOCUMENTS.
 REINFORCING STEEL - PLACE AND TIE ALL REINFORCING STEEL IN ACCORDANCE WITH THE GEORGIA DOT SPECIFICATIONS. DO NOT WELD REINFORCING STEEL.
 STATIONS AND OFFSETS - STATIONS SHOWN ARE ALONG CL WESLEY CHAPEL RD EB OFF RAMP. OFFSETS SHOWN ARE MEASURED TO THE FRONT FACE OF WALL.
 CHAMFER - CHAMFER ALL EXPOSED CONCRETE EDGES 3/4 INCH UNLESS OTHERWISE NOTED.
 JOINTS IN WALL FORMS - JOINTS IN WALL FORMS SHALL BE SUPPORTED WITH BACKING STRIPS TO PROVIDE A FLUSH SURFACE IN THE FINISHED WALL FACE.
 FINISH - EXPOSED SURFACE OF THE WALL SHALL RECEIVE AN ASHLAR STONE FINISH.
 CONCRETE FACING - WALL FACING IS 12 INCH THICK CAST IN PLACE REINFORCED CONCRETE. PNEUMATICALLY APPLIED CONCRETE (SHOTCRETE) WILL NOT BE ALLOWED AS A PERMANENT WALL FACING.
 WATERSTOPS - ALL VERTICAL EXPANSION JOINTS SHALL CONTAIN A 6" WATERSTOP GEOCOMPOSITE WALL DRAIN - USE A GEOCOMPOSITE WALL DRAIN, SELECTED FROM THE GEORGIA DOT QUALIFIED PRODUCTS LIST, BEHIND THE WALL FACING AT WEEP HOLE LOCATIONS.
 CONCRETE COVER - MAINTAIN 2 INCHES COVER ON ALL REINFORCING STEEL.
 WEEP HOLES - 4" I.D. WEEP HOLES SHALL BE PROVIDED AT A MAXIMUM SPACING OF 8' O.C., FULL LENGTH OF WALL.
 WELDING - ALL WELDING ON GEORGIA DOT PROJECTS SHALL BE PERFORMED BY CERTIFIED WELDERS THAT HAVE IN THEIR POSSESSION A CURRENT WELDING CERTIFICATION CARD ISSUED BY THE OFFICE OF MATERIALS AND RESEARCH. USE ONLY E70XX (EXCLUDING E7014 AND E7024) LOW HYDROGEN ELECTRODES FOR MANUAL SHIELDED METAL ARC WELDING.
 WALL PLANS - THE RETAINING WALL IS CONSIDERED A CONTRACTOR DESIGN. THESE WALL PLANS ARE CONCEPTUAL AND ARE FOR ILLUSTRATIVE PURPOSES ONLY. EXACT NUMBER OF H-PILES, THEIR LOCATIONS AND DESIGN LOADS SHALL BE PROVIDED BY THE CONTRACTOR FOR THE WALL SYSTEM BID. THE PRESENCE OF THESE CONCEPTUAL PLANS IN THE CONTRACT DOCUMENTS IN NO WAY RELIEVES THE CONTRACTOR FROM PROVIDING A WALL SYSTEM WHICH PROVIDES STRUCTURAL ADEQUACY, INCLUDING H-PILE TESTING, IN ACCORDANCE WITH THE GEORGIA DOT SPECIFICATIONS AND SPECIAL PROVISION 628, AT THE BID PRICE.
 INCIDENTAL ITEMS - INCLUDE THE COST INCIDENTAL TO THE WORK THAT IS NOT SPECIFICALLY COVERED BY THE GEORGIA STANDARD SPECIFICATIONS, SUPPLEMENTAL SPECIFICATIONS AND/OR SPECIAL PROVISIONS IN THE OVERALL BID SUBMITTED. THIS INCLUDES THE COST OF JOINT FILLERS AND OTHER INCIDENTAL ITEMS NECESSARY TO COMPLETE THE WORK.

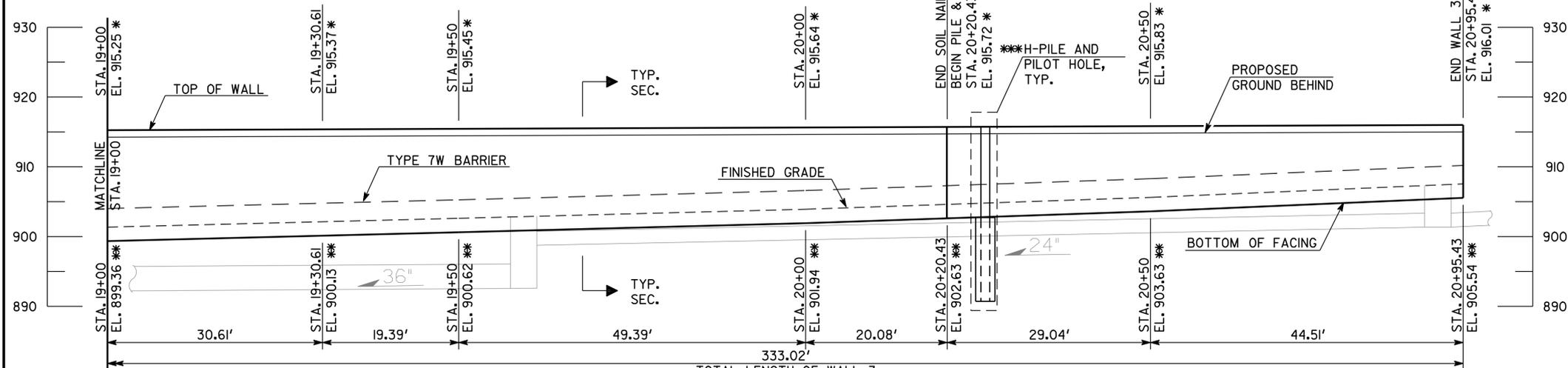


PLAN



TYPICAL SECTION
PILE AND LAGGING WALL

WALL NO. 3

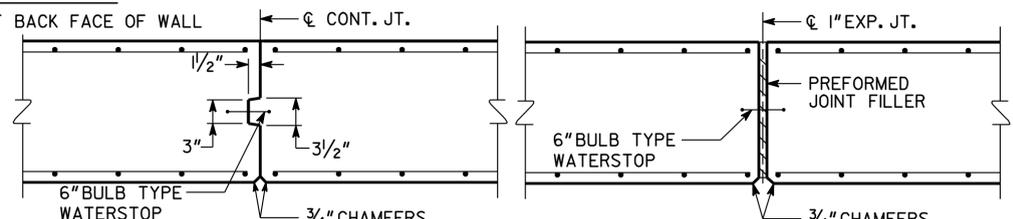


HORIZONTAL CURVE DATA

CURVE KC41
 P.I. STA. 20+82.49
 $\Delta = 11^\circ-29'-46''$
 $D = 4^\circ-48'-53''$
 $R = 1190.00'$
 $T = 119.79'$
 $L = 238.77'$

TOTAL LENGTH OF WALL 3
ELEVATION

LOOKING AT BACK FACE OF WALL



DETAIL OF CONTRACTION JOINT

DETAIL OF EXPANSION JOINT

- NOTES:
 1. * ELEVATION OF TOP OF WALL AT BACK FACE, TYP.
 2. ** ELEVATION AT BOTTOM OF FACING, TYP.
 3. *** SIZE, QUANTITY, AND LOCATION TO BE GIVEN IN SHOP DRAWINGS.
 4. WALL 3 FINISH TO MATCH FINISH OF EXISTING MSE WALL.

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 A UNIT OF MICHAEL BAKER CORPORATION

GEORGIA
DEPARTMENT OF TRANSPORTATION
 ENGINEERING DIVISION-OFFICE OF BRIDGES AND STRUCTURES

WALL PLAN AND ELEVATION - SOIL NAIL/
 PILE & LAGGING
 I-20 CD LANES

DEKALB COUNTY 0009542

SCALE: 1" = 10'-0" DECEMBER 2011

DRAWING NO.	31-12
WALL SHEET	2 OF 2

DESIGNED	BSB	CHECKED	ANZ	REVIEWED	WEI/WMD
DRAWN	BSB	DESIGN GROUP	AWB	APPROVED	BFR