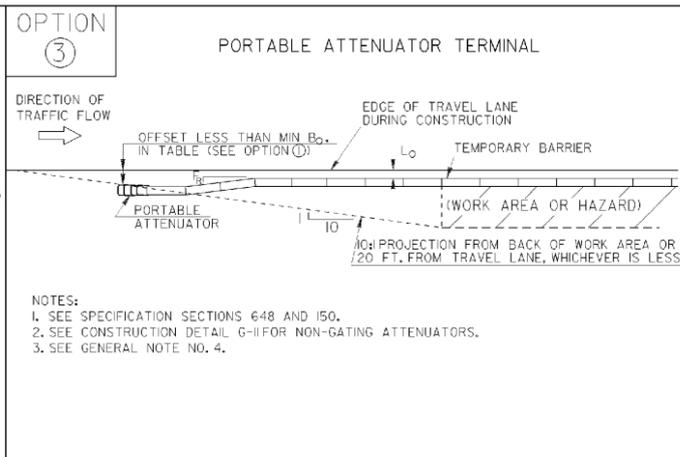
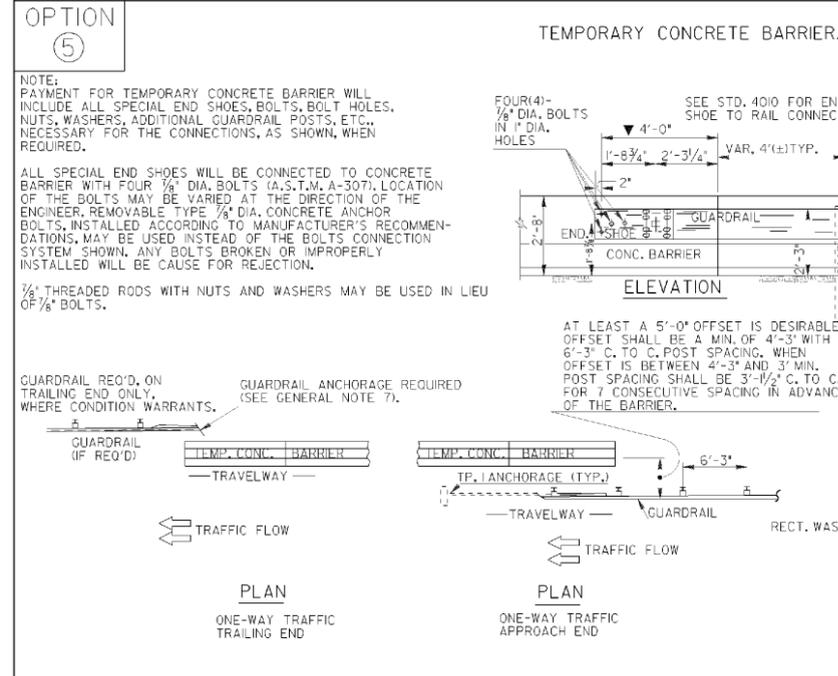
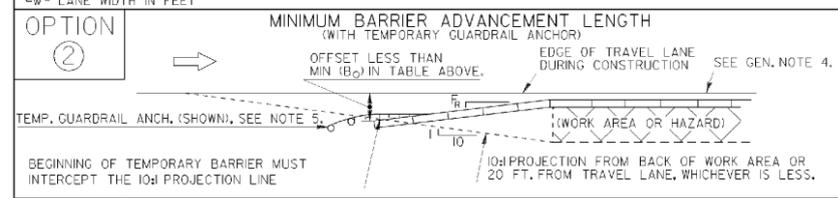
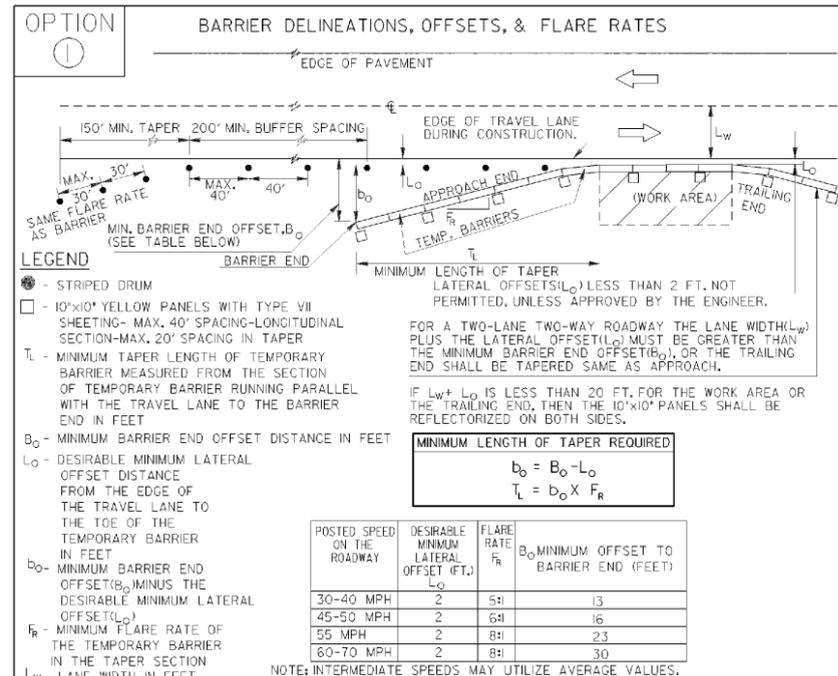
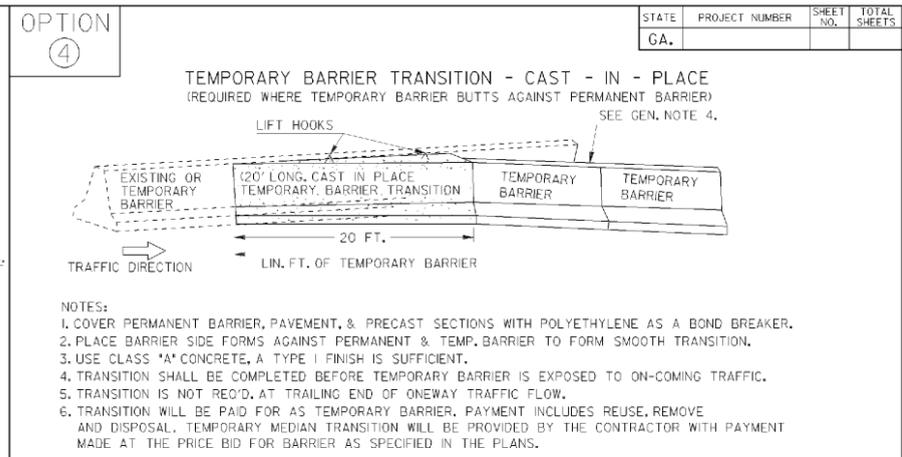


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NOTES:
1. SEE SPECIFICATION SECTIONS 648 AND 150.
2. SEE CONSTRUCTION DETAIL G-11 FOR NON-GATING ATTENUATORS.
3. SEE GENERAL NOTE NO. 4.



NOTES:
1. COVER PERMANENT BARRIER, PAVEMENT, & PRECAST SECTIONS WITH POLYETHYLENE AS A BOND BREAKER.
2. PLACE BARRIER SIDE FORMS AGAINST PERMANENT & TEMP. BARRIER TO FORM SMOOTH TRANSITION.
3. USE CLASS "A" CONCRETE. A TYPE 1 FINISH IS SUFFICIENT.
4. TRANSITION SHALL BE COMPLETED BEFORE TEMPORARY BARRIER IS EXPOSED TO ON-COMING TRAFFIC.
5. TRANSITION IS NOT REQ'D. AT TRAILING END OF ONEWAY TRAFFIC FLOW.
6. TRANSITION WILL BE PAID FOR AS TEMPORARY BARRIER. PAYMENT INCLUDES REUSE, REMOVE AND DISPOSAL. TEMPORARY MEDIAN TRANSITION WILL BE PROVIDED BY THE CONTRACTOR WITH PAYMENT MADE AT THE PRICE BID FOR BARRIER AS SPECIFIED IN THE PLANS.

END TREATMENT FOR BARRIER TERMINAL

WHERE BARRIER END OFFSETS REQUIRED IN OPTION 1 CAN NOT BE OBTAINED THEN, A TEMPORARY GUARDRAIL ANCHORAGE AT THE BARRIER END WILL BE REQUIRED AS PER OPTION 2. IF INSTALLATION OF GUARDRAIL ANCHORAGE IS NOT PRACTICAL DUE TO LOCAL CONDITIONS, A TEMPORARY PORTABLE IMPACT ATTENUATOR SHOULD BE USED AT THE BARRIER END AS PER OPTION 3.

1 = FIRST OPTION, MINIMUM OFFSET IS OBTAINED.
2 = SECOND OPTION, TEMPORARY GUARDRAIL ANCHORAGE, AS SPECIFIED.
3 = THIRD OPTION, PORTABLE ATTENUATOR.
4 = FOURTH OPTION, TERMINATE WITH A CAST-IN-PLACE TRANSITION.
5 = FIFTH OPTION, CONNECT TO GUARDRAIL.

GENERAL NOTES:

- BARRIERS SHALL BE PLACED SUCH THAT OPENINGS BETWEEN INDIVIDUAL SECTIONS SHALL BE KEPT TO A MAXIMUM. ALL JOINTS BETWEEN PRECAST SECTIONS SHALL BE CONNECTED AS REQUIRED BY GA. STANDARD 4361 AND/OR MANUFACTURERS RECOMMENDATIONS.
- THE BARRIER IS NOT TO BE CONNECTED TO THE BRIDGE DECK BY CONNECTING PINS OR REBAR UNLESS AN APPROVED METHOD FOR CONNECTION IS REQUIRED.
- PRECAST BARRIER SECTIONS SHALL CONFORM TO THE DIMENSIONAL REQUIREMENTS IN GA. STANDARD 4361 AND/OR MANUFACTURERS RECOMMENDATIONS.
- TRAFFIC CONTROL NOT SHOWN ON THIS STANDARD SHALL BE IN ACCORDANCE WITH CURRENT EDITION OF SECTION 150, STANDARDS, CONSTRUCTION DETAILS AND/OR PLAN SHEETS. DRUMS, ETC. SHOWN FOR OPTION 1 SHALL BE APPLICABLE FOR THE OTHER OPTIONS AS WELL. DRUMS SHALL BE PROVIDED IN ADVANCE OF TEMPORARY BARRIERS INSTALLATION AND SHALL BE IN PLACE BEFORE THE BARRIER IS INSTALLED AND REMOVED AFTER THE BARRIER IS REMOVED.
- WHERE TEMPORARY GUARDRAIL ANCHORAGE IS SPECIFIED, THE FIRST BREAKAWAY SHALL BE 37'-6" MINIMUM IN ADVANCE OF BEGINNING THE TEMPORARY BARRIER INSTALLATION.
- PAYMENT FOR TEMPORARY GUARDRAIL ANCHORS INCLUDES END SHOE, CONNECTING BOLTS, NUTS AND WASHERS, ADDITIONAL POSTS AND OFFSETS BLOCKS, 25 FT. ADDITIONAL "W" BEAM WITH THE DOUBLE NESTED SECTION PLUS STANDARD ANCHORAGE COMPONENTS. (TOTAL LENGTH=62'-6" (+) MIN.
- UNACCEPTABLE OR NON-STANDARD END TREATMENT WILL NOT BE LEFT IN PLACE AFTER REMOVAL OF THE TEMPORARY BARRIER, ALL GUARDRAIL AND ALL ANCHORAGES LEFT IN PLACE WILL BE TREATED AS NECESSARY TO CONFORM WITH CURRENT STANDARDS IMMEDIATELY AFTER REMOVAL OF THE TEMPORARY BARRIER.

5-10-07	2-1-01	DATE	DEPARTMENT OF TRANSPORTATION STATE OF GEORGIA
REVISED ALL OPTIONS AND REV. REFLECTIVE SHEETING TYPE AND GENERAL NOTES.		REVISION	STANDARD
GLO	RML	BY	TEMPORARY BARRIER (END TREATMENT OPTIONS)
		NO SCALE	REV. & REDR. MAY, 1999
		DES. (SUBMITTED)	NUMBER 4960
		DRW. (STATE ROAD & AIRPORT DESIGN ENGINEER)	
		TRN. (APPROVED)	
		CHK. (CHIEF ENGINEER)	

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REVISION DATES	