

CR467/BICKLEY HIGHWAY
STA 12+00 - 21+50 (RT)
STA 29+50 - 36+00 (LT)

REQUIRED PAVEMENT

- (A) RECYCLED ASPH CONC 9.5 MM SUPERPAVE, TYPE 1, GP 1 OR BLEND 1, INCL BITUM MATL & H LIME
- (B) RECYCLED ASPH CONC 19 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME
- (C) GR AGGR BASE CRS, 8 INCH, INCL MATL
- (D) INDENTATION RUMBLE STRIPS - GROUND-IN-PLACE (SKIP) MILLED RUMBLE STRIPS SHALL BE PLACED IN 28 FOOT SEGMENTS SEPARATED BY 12 FOOT GAPS. RUMBLE STRIPS SHALL BE OMITTED AT ALL DRIVEWAYS.

△ ALGEBRAIC DIFFERENCE IN PAVING & SHOULDER SLOPE NOT TO EXCEED 8%

SLOPE CONTROL BOX		
SLOPE	CUT	FILL
4:1	0'-6'	0'-7'
3:1	6'-10'	----
2:1	>10'	>7'

SEE PLANS FOR EXCEPTIONS
* REQUIRES GUARDRAIL

ALLOWABLE RANGES TABLE

FOR THIS PROJECT, CROSS SLOPES THAT ARE ADJUSTED TO "BEST FIT" EXISTING PAVEMENT SLOPES ARE SUBJECT TO THE FOLLOWING LIMITS:

A. NORMAL CROWN

SECTION WITH GRADES 0.5% OR GREATER	SECTION WITH GRADES LESS THAN 0.5%
1.50 % - MINIMUM	1.56 % - MINIMUM
2.08 % - DESIRABLE	2.08 % - DESIRABLE
2.50 % - MAXIMUM	3.00 % - MAXIMUM

B. SUPERELEVATION RATE

S.E. RATE SHOWN ON PLANS OR SE RATE EXISTING IN FIELD, WHICHEVER IS GREATER.

C. SUPERELEVATION TRANSITION LENGTH (LENGTH FROM FLAT POINT TO FULL SE)

	RATE OF CHANGE	CORRESPONDING DIFFERENCE IN GRADE BETWEEN PIVOT POINT AND EDGE OF PAVEMENT
MINIMUM	1:150	0.67%
DESIRABLE	1:200	0.50%
MAXIMUM	1:300	0.33%

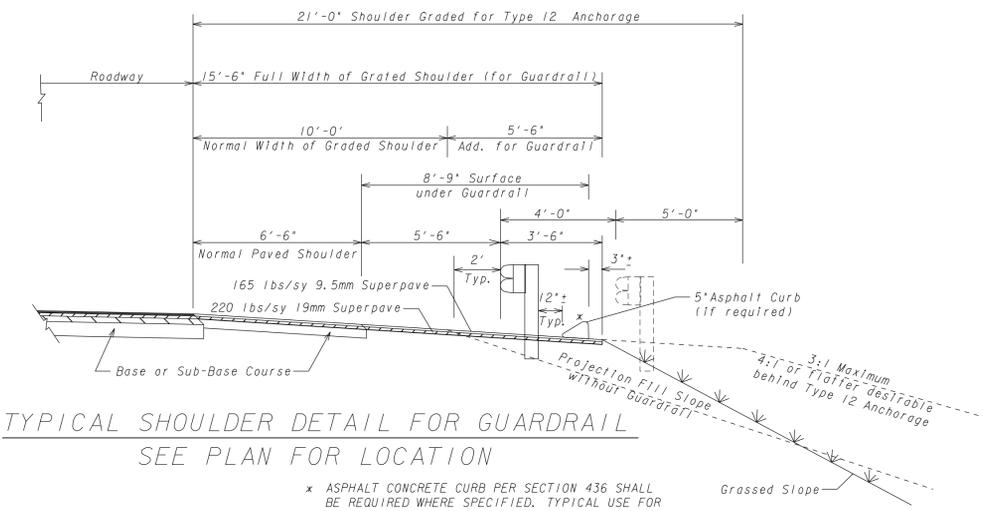
LENGTH SHALL BE SET TO AVOID CREATING A FLAT GUTTER GRADE ON LOW SIDE AND TO AVOID FLAT CROSS SLOPES AT OR NEAR THE LOW POINT OF VERTICAL CURVES.

D. POSITIONING OF SUPERELEVATION TRANSITION LENGTH ON SIMPLE CURVES

- 50% OF TRANSITION INSIDE CURVE - MAXIMUM
- 33% OF TRANSITION INSIDE CURVE - DESIRABLE
- 20% OF TRANSITION INSIDE CURVE - MINIMUM

NOTE: CROWN WIPE-OUT SHALL BE AT THE SAME RATE AS THE SE TRANSITION.

E. SMOOTHING OF BREAKS IN EDGE PROFILE AT BEGIN AND END OF TRANSITION SHALL BE ACCOMPLISHED BY VERTICAL CURVE WITH A MINIMUM LENGTH (IN FEET) EQUAL TO THE SPEED DESIGN (IN MPH).



* ASPHALT CONCRETE CURB PER SECTION 436 SHALL BE REQUIRED WHERE SPECIFIED. TYPICAL USE FOR EROSION PREVENTION OF HIGH FILL SLOPES. WHERE SPILLWAYS ARE REQUIRED, ASPHALT CONCRETE CURB SHALL NOT BE USED WITHIN THE LIMITS OF THE TYPE 12 ANCHORAGE.



REVISION DATES		STATE OF GEORGIA DEPARTMENT OF TRANSPORTATION	
12/31/10		OFFICE:	
01/18/11		TYPICAL SECTIONS	
		CR467/BICKLEY HWY AT CANE CREEK BRIDGE REPLACEMENT	
		CSBRG-0006-00(441) 10/15/2010	

DRAWING No.
5-01