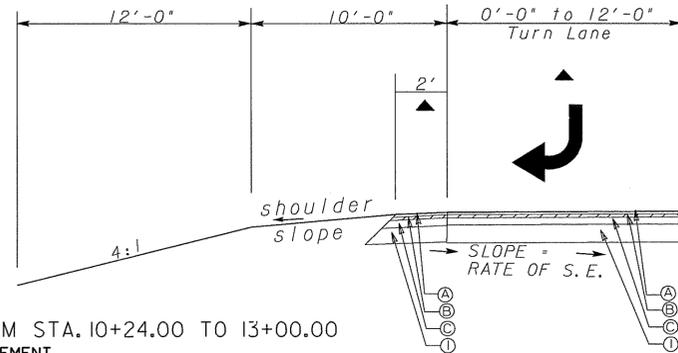


**TURN LANE TYPICAL SECTION
ACCESS TO SR 67 FROM SR 46**



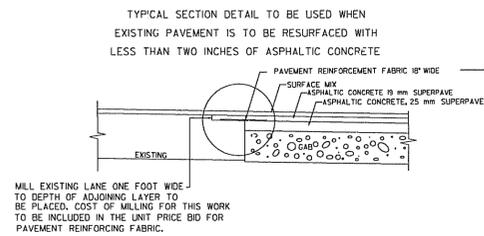
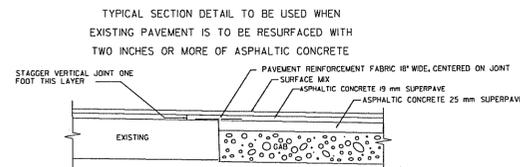
APPLIES FROM STA. 10+24.00 TO 13+00.00

REQUIRED PAVEMENT

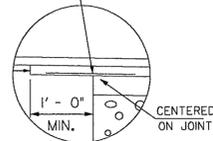
- Ⓐ 9.5 mm ASPH. CONC. SUPERPAVE, 135 LBS./SQ.YD.
- Ⓑ 19.0 mm ASPH. CONC. SUPERPAVE, 220 LBS./SQ.YD.
- Ⓒ 25.0 mm ASPH. CONC. SUPERPAVE, 440 LBS./SQ.YD.
- Ⓓ GRADED AGGREGATE BASE 8"
- Ⓔ GRADED AGGREGATE BASE 12"
- Ⓕ LEVELING (As Required)
- Ⓖ GRADED AGGREGATE BASE 6"
- Ⓗ GRADED AGGREGATE BASE 10"

S. E. RATE	shoulder slope
2.0% OR 3.0%	4.0%
4.0% OR 5.0%	2.0%
6.0% OR 7.0%	1.0%
8.0% +	0.0%

PAVEMENT REINFORCING FABRIC

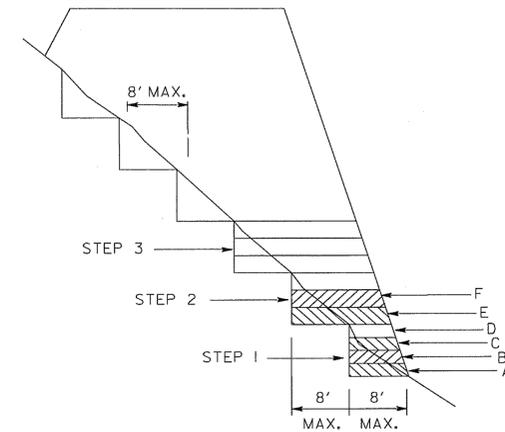


MILL EXISTING LANE ONE FOOT WIDE TO DEPTH OF ADJOINING LAYER TO BE PLACED. COST OF MILLING FOR THIS WORK TO BE INCLUDED IN THE UNIT PRICE BID FOR PAVEMENT REINFORCING FABRIC.



VOID

BENCHING DETAIL



- WHERE THE EMBANKMENT IS TO BE PLACED ON A HILLSIDE OR ANOTHER EXISTING EMBANKMENT HAVING A SLOPE OF 3 TO 1 OR STEEPER, THE FOUNDATION MUST BE BENCHED WHILE THE EMBANKMENT IS BEING MADE. (SEE DIAGRAM ABOVE)
- THE DIAGRAM SHOWS THAT BEFORE LAYER "A" IS PLACED THE FIRST STEP (1) IS CUT INTO THE SLOPE A MAXIMUM DISTANCE OF ABOUT 8 FEET (ABOUT 3/4 THE WIDTH OF THE TYPICAL D-8 BULLDOZER BLADE). SUCCESSIVE LAYERS B, C, AND D ARE THEN PLACED. BEFORE LAYER "E" IS PLACED, THE SECOND STEP IS CUT 8 FEET INTO THE SLOPE AND SUCCESSIVE LAYERS ARE AGAIN PLACED. IF IT IS ANTICIPATED THAT THE VERTICAL PART OF THE STEP WILL EXCEED 4 FEET IF A 8 FEET HORIZONTAL CUT IS MADE, THEN THE ACTUAL CUT STOPS WHEN THE VERTICAL PART REACHES A MAXIMUM OF 4 FEET ALLOWING THE HORIZONTAL DISTANCE TO VARY.
- THE PROCESS OF BENCHING IS CONSIDERED INCIDENTAL TO THE ITEM OF UNCLASSIFIED EXCAVATION AND BORROW OR GRADING COMPLETE IN CONSTRUCTION OF THE EMBANKMENT AND NO ADDITIONAL MEASUREMENT OF QUANTITY OR PAYMENT WILL BE MADE FOR BENCHING.

▲ PAY AREA FOR GRADED AGGR. BASE

GEORGIA
DEPARTMENT
OF
TRANSPORTATION

REVISION DATES

NO.	DATE	DESCRIPTION

STATE OF GEORGIA
DEPARTMENT OF TRANSPORTATION
OFFICE: JESUP ROAD DESIGN
TYPICAL SECTIONS

DRAWING No.
5-7