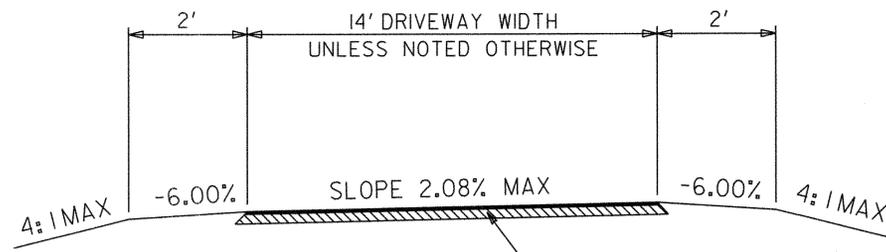


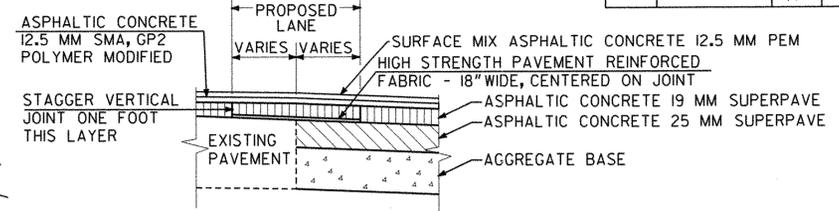
TYPICAL SHOULDER DETAIL FOR GUARDRAIL
SEE PLAN FOR LOCATION
(SEE GDOT CONSTRUCTION DETAIL S-4B ONLY)



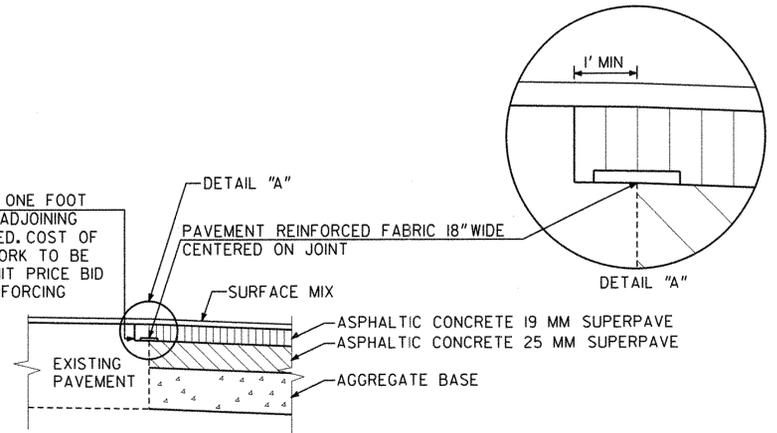
DRIVEWAY TYPICAL SECTION
DRIVEWAY REPLACEMENT DATA

ALL DRIVEWAYS, WHERE ACCESS IS ALLOWED, SHALL BE PLACED AS DIRECTED BY THE ENGINEER IN ACCORDANCE WITH RULES AND REGULATIONS FOR CONTROL AND PROTECTION OF DOT RIGHTS OF WAY. ALL DRIVEWAYS THAT ARE TO BE RECONSTRUCTED SHALL BE REPLACED IN KIND, I.E., ASPHALT FOR ASPHALT, CONCRETE FOR CONCRETE. FOR EARTH DRIVES, ASPHALT PAVING WILL BE REQUIRED UP TO THE R/W LINE. AGGREGATE SURFACE COURSE WILL BE USED OTHERWISE. THE DRIVEWAY LOCATIONS INDICATED ON THE PLANS ARE FROM THE BEST AVAILABLE DATA. THE CONTRACTOR SHALL CONSTRUCT NEW DRIVEWAYS TO MATCH THE ACTUAL FIELD LOCATION OF EXISTING DRIVEWAYS WHERE THEY ARE NOT IN CONFLICT WITH THE RULES AND REGULATIONS. THE CONTRACTOR SHALL OBTAIN THE APPROVAL OF THE ENGINEER PRIOR TO MAKING ANY REVISIONS SUCH AS TO LOCATION, WIDTH AND/OR NUMBER OF DRIVES TO BE CONSTRUCTED. WHERE REQUIRED, THE DRIVES SHALL BE PAVED AS FOLLOWS:

ASPHALTIC DRIVES
RESIDENTIAL - ASPHALTIC CONCRETE 9.5 mm SUPERPAVE (135 lb/sy)
- ASPHALTIC CONCRETE 19mm SUPERPAVE (330 lbs/sy)

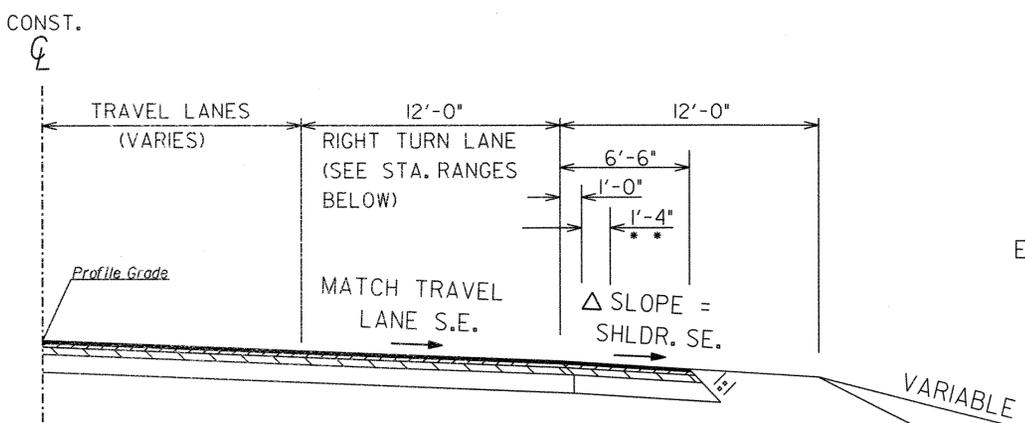


TYPICAL SECTION DETAIL TO BE USED WHEN EXISTING PAVEMENT IS TO BE RESURFACED WITH 2 INCHES OR MORE OF ASPHALTIC CONCRETE.



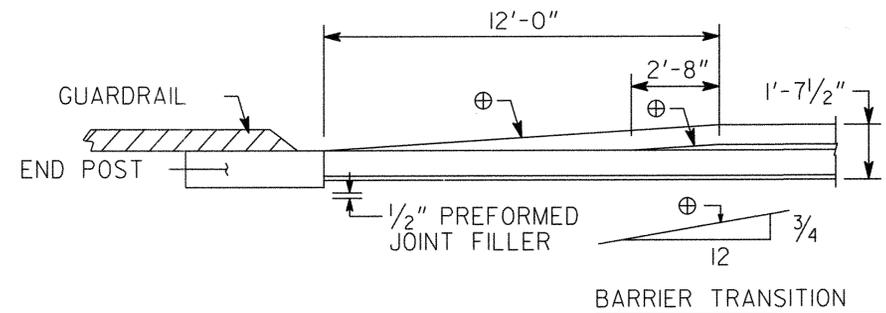
TYPICAL SECTION DETAIL TO BE USED WHEN EXISTING PAVEMENT IS TO BE RESURFACED WITH LESS THAN 2 INCHES OF ASPHALTIC CONCRETE.

NOTE: STANDARD CROSS SLOPE OF 2.00% MAY BE VARIED AS DIRECTED BY THE ENGINEER TO BEST FIT THE EXISTING ROADWAY AS PER SPEC. 149 OF THE SUPPLEMENTAL SPECIFICATIONS. SEE "ALLOWABLE RANGES TABLE".



RIGHT TURN LANE DETAIL

APPLIES TO MAINLINE STA. 2031+31.28 TO STA. 2035+48.25 (NBL) AND STA. 2047+90.34 TO STA. 2051+10.48 (SBL)
APPLIES TO RAMP B STA. 219+01.07 TO STA. 220+61.18 (RIGHT)
APPLIES TO RAMP D STA. 411+20.00 TO STA. 413+30 (RIGHT)



BARRIER TRANSITION DETAIL

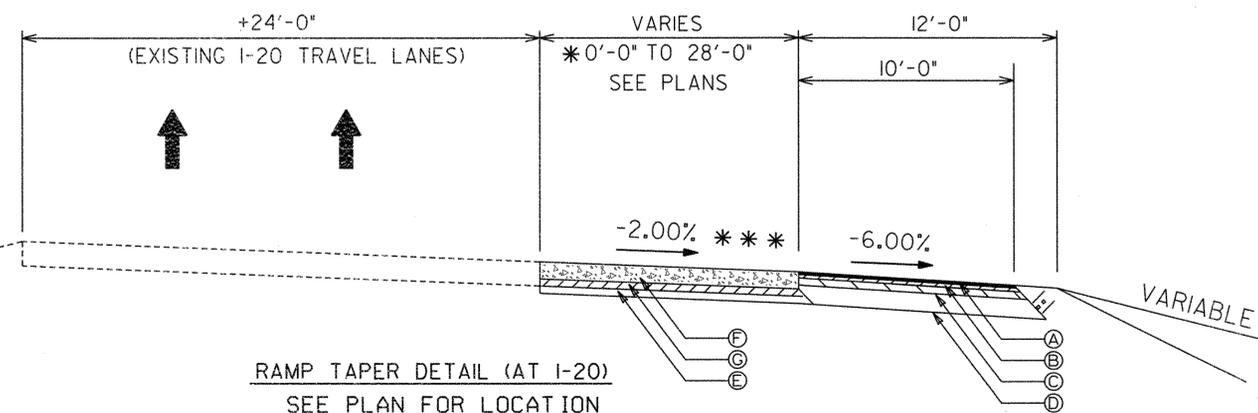
** INDENTATION RUMBLE STRIPS REQUIRED (SKIP) GA DETAIL S-8

SLOPE CONTROLS		
SLOPE	CUT	FILL
4:1	0-6'	0-10'
3:1	6'-10'	--
2:1*	OVER 6'	OVER 6'

* GUARDRAIL REQUIRED

REQUIRED PAVEMENT

- (A) ASPHALTIC CONCRETE, 9.5 mm SUPERPAVE TYPE II, 135 lbs/sq. yd.
- (B) ASPHALTIC CONCRETE, 19 mm SUPERPAVE, 220 lbs/sq. yd.
- (C) ASPHALTIC CONCRETE, 25 mm SUPERPAVE, 440 lbs/sq. yd.
- (D) GRADED AGGREGATE BASE, 8"
- (E) GRADED AGGREGATE BASE, 12"
- (F) PLAIN PORTLAND CEMENT PAVEMENT, CLASS 3 CONCRETE, 12"
- (G) ASPHALTIC CONCRETE, 25 mm SUPERPAVE, 330 lbs/sq. yd.
- (H) GRADED AGGREGATE BASE, 6"



RAMP TAPER DETAIL (AT I-20)
SEE PLAN FOR LOCATION

*** SEE GEORGIA CONSTRUCTION DETAILS FOR ENTRANCE AND EXIT RAMP DIMENSIONS AND SUPERELEVATION INFORMATION

ALLOWABLE RANGES TABLE		
FOR THIS PROJECT, CROSS SLOPES THAT ARE ADJUSTED TO "BEST FIT" EXISTING PAVEMENT SLOPES ARE SUBJECT TO THE FOLLOWING LIMITS:		
A. NORMAL CROWN		
SECTION WITH GRADES 0.5% OR GREATER	SECTION WITH GRADES LESS THAN 0.5%	
1.50 PERCENT - MINIMUM	1.56 PERCENT - MINIMUM	
2.00 PERCENT - DESIRABLE	2.00 PERCENT - DESIRABLE	
2.50 PERCENT - MAXIMUM	3.0 PERCENT - MAXIMUM	
B. SUPERELEVATION RATE		
S.E. RATE SHOWN ON PLANS OR SE RATE EXISTING IN FIELD, WHICHEVER IS GREATER.		
C. SUPERELEVATION TRANSITION LENGTH (LENGTH FROM FLAT POINT TO FULL SE)		
	RATE OF CHANGE	CORRESPONDING DIFFERENCE IN GRADE BETWEEN PIVOT POINT AND EDGE OF PAVEMENT
MINIMUM	1:150	0.67%
DESIRABLE	1:200	0.50%
MAXIMUM	1:300	0.33%
LENGTH SHALL BE SET TO AVOID CREATING A FLAT GUTTER GRADE ON LOW SIDE AND TO AVOID FLAT CROSS SLOPES AT OR NEAR THE LOW POINT OF VERTICAL CURVES.		
D. POSITIONING OF SUPERELEVATION TRANSITION LENGTH ON SIMPLE CURVES		
50% OF TRANSITION INSIDE CURVE - MAXIMUM		
33% OF TRANSITION INSIDE CURVE - DESIRABLE		
20% OF TRANSITION INSIDE CURVE - MINIMUM		
NOTE: CROWN WIPE-OUT SHALL BE AT THE SAME RATE AS THE SE TRANSITION.		
E. SMOOTHING OF BREAKS IN EDGE PROFILE AT BEGIN AND END OF TRANSITION SHALL BE ACCOMPLISHED BY VERTICAL CURVE WITH A MINIMUM LENGTH (IN FEET) EQUAL TO THE SPEED DESIGN (IN MPH).		

NOT TO SCALE

REVISIONS					
Δ	DATE	BY	DESCRIPTION	Δ	DATE

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Norcross, Georgia 30071
Telephone (770) 263-5945

DESIGNED BY	NAME	DATE	DRAWN BY	NAME	DATE	QUANTITIES CHECKED BY	NAME	DATE

Department of Transportation
State of Georgia

TYPICAL SECTION